

Strategic Planning Board

Agenda

Date:	Friday, 28th June, 2024
Time:	10.00 am
Venue:	Council Chamber, Municipal Buildings, Earle Street, Crewe CW1 2BJ

The agenda is divided into 2 parts. Part 1 is taken in the presence of the public and press. Part 2 items will be considered in the absence of the public and press for the reasons indicated on the agenda and at the foot of each report.

It should be noted that Part 1 items of Cheshire East Council decision making meetings are audio recorded and the recordings will be uploaded to the Council's website

PART 1 – MATTERS TO BE CONSIDERED WITH THE PUBLIC AND PRESS PRESENT

1. **Apologies for Absence**

To receive any apologies for absence from Members.

2. **Declarations of Interest/Pre Determination**

To provide an opportunity for Members and Officers to declare any disclosable pecuniary interests, other registerable interests, and non-registerable interests in any item on the agenda and for Members to declare if they have a pre-determination in respect of any item on the agenda.

3. **Minutes of the Previous Meeting** (Pages 3 - 14)

To approve the minutes of the meeting held on 29 May 2024 as a correct record.

For requests for further information

Contact: Sam Jones

Tel: 01270 686643

E-Mail: samuel.jones@cheshireeast.gov.uk with any apologies

4. **Public Speaking**

A total period of 5 minutes is allocated for each of the planning applications for the following:

- Ward Councillors who are not members of the Strategic Planning Board
- The relevant Town/Parish Council

A period of 3 minutes is allocated for each of the planning applications for the following individuals/groups:

- Members who are not members of the Strategic Planning Board and are not the Ward Member
- Objectors
- Supporters
- Applicants

5. **22/2476N - Land Off Minshull New Road and FLOWERS LANE, LEIGHTON** (Pages 15 - 40)

To consider the above planning application.

6. **22/2499N - Land Off Minshull New Road and FLOWERS LANE, LEIGHTON** (Pages 41 - 78)

To consider the above planning application.

7. **22/2500N - Land Off Minshull New Road and FLOWERS LANE, LEIGHTON** (Pages 79 - 104)

To consider the above planning application.

8. **22/3228N - Land Off Minshull New Road and FLOWERS LANE, LEIGHTON** (Pages 105 - 126)

To consider the above planning application.

9. **22/3158N - LAND WEST OF DAVID WHITBY WAY, CREWE** (Pages 127 - 158)

To consider the above planning application.

10. **23/2945M - Land Off, LEEK ROAD, BOSLEY** (Pages 159 - 184)

To consider the above planning application.

Membership: Councillors R Bailey, J Bratherton, S Edgar, K Edwards, S Gardiner (Vice-Chair), M Gorman, T Jackson, G Marshall, H Moss, B Puddicombe (Chair) and L Smetham

CHESHIRE EAST COUNCIL

Minutes of a meeting of the **Strategic Planning Board**
held on Wednesday, 29th May, 2024 in the The Capesthorne Room - Town
Hall, Macclesfield SK10 1EA

PRESENT

Councillor S Gardiner (Vice-Chair, in the Chair)

Councillors S Edgar, K Edwards, M Gorman, T Jackson, G Marshall, H Moss,
L Smetham, L Braithwaite, J Bratherton and A Harrison

OFFICERS IN ATTENDANCE

David Malcolm, Head of Planning
Nick Hulland, Principal Planning Officer
Richard Taylor, Principal Planning Officer
Paul Griffiths, Major Projects Officer
James Thomas, Senior Planning and Highways Lawyer
Sam Jones, Democratic Services Officer

1 APOLOGIES FOR ABSENCE

Apologies for absence were received from Councillor B Puddicombe. Councillor L Braithwaite was present on behalf of Councillor B Puddicombe.

2 DECLARATIONS OF INTEREST/PRE DETERMINATION

In the interest of openness the following declarations were made:

Councillor S Edgar declared that, in relation to application 18/3672M, he was known to the applicant, but had not discussed the application with them.

Councillor S Edgar declared that, in relation to application 18/3672M, he and other Members of the Strategic Planning Board had received a third-party email.

Councillor A Harrison declared that, in relation to application 18/3672M, he was known to applicant as the applicant is a significant landowner in their Ward.

Councillor H Moss declared that, in relation to application 18/3672M, she knew the applicant and sees them at social events.

Councillor S Gardiner declared that, in relation to application 18/3672M, he was known to the landowner, and had discussed the logistics of the application, but not the merit of it or otherwise.

Councillor S Gardiner declared that, in relation to application 18/3672M, he was known to Debbie Jamison (objector).

Councillor S Edgar declared that, in relation to application 23/0539N, he had an interest in the application and would speak to it during the Public Speaking section and then vacate the meeting for that item,

3 MINUTES OF THE PREVIOUS MEETING

RESOLVED:

That the minutes of the meeting held on 24 April 2024 be approved as a correct record.

4 PUBLIC SPEAKING

RESOLVED:

The public speaker procedure was noted.

5 **18/3672M - TATTON BLUEBELL VILLAGE, LAND EAST OF MANCHESTER ROAD, KNUTSFORD, WA16 0NS**

Consideration was given to the above planning application.

The following attended the meeting and spoke in relation to the application:

Ward Councillor: Councillor Tony Dean

Parish Councillor: Councillor James McCulloch, Knutsford Town Council

Objectors: Martyn Symes, Debbie Jamison

Agent / Applicant: Amy Bowden

RESOLVED:

For the reasons set out in the report, and the update report, the application be APPROVED as RECOMMENDED, subject to the following conditions and informatives:

1. Outline (commencement of development)
2. Requirement to submit Reserved Matters application(s)
3. Time limit on submission of Reserved Matters
4. Approved plan/s
5. Submission/approval of a Phasing Plan
6. The development shall include a total of up to 275 residential units (Use Class C2 and C3) of which no more than 250 shall fall within C3 Use Class. C2 units to be no more than 50.
7. The proposed 'Local Centre' shall comprise of no more than 1,000m² gross of commercial floorspace. No individual unit shall be more than 450m² gross. The convenience goods floorspace in any one retail unit should not exceed 280m² net.
8. 'Local Centre' shall include the following uses only: Class E(a), E(b), E(c), sui generis (takeaway(s)) all within the 1000m², E(e) (800m²) and at first-floor level or above Office (E(gi)) or residential (C3).

9. Submission/approval of an Access Statement including details of; primary access, any secondary/emergency access, a suitable crossing on Manchester Road, & scope for speed restrictions
10. Submission/approval of bespoke Spatial Design Code(s)
11. Submission/approval of a scheme of Archaeological investigation
12. Submission/approval of an updated Landscape & Visual Appraisal
13. Submission/approval of detailed Landscaping Scheme informed by an updated Landscape & Visual Appraisal (including retention of existing and replacement hedgerow planting)
14. The landscape buffers as required by LPS36 shall be provided.
15. Submission/approval of existing and proposed ground spot levels and proposed finished floor levels.
16. Submission/approval of Arboricultural Report(s)
17. Submission/approval of a detailed drainage design for the entirety of the outline consent
18. Submission/approval of a drainage management and maintenance plan
19. Submission/approval of Construction Environmental Management Plan(s)
20. Submission/approval of a bat survey should any trees within that phase be identified for removal.
21. Prior to commencement of development, the consented development be entered into Natural England's DLL scheme for Great Crested Newts.
22. Submission/approval of a biodiversity/ecological enhancement strategy
23. Protection of nesting and breeding birds
24. Submission/approval of an external lighting scheme
25. Submission/approval of a habitat creation method statement, ecological monitoring strategy and a 30-year habitat management plan for retained, enhanced and newly created habitats.
26. The woodland planting proposed within the blue edge (off-site) be delivered as part of the implementation of the first phase.
27. Submission/approval of an acoustic report demonstrating how the detailed scheme will achieve the requirements for both internal and external noise.
28. Submission/approval of an acoustic validation report prior to occupation of each phase proposing residential development.
29. Submission/approval of a Phase II ground investigation and risk assessment
30. Submission/approval of a contaminated land verification report
31. Submission/approval of an imported soil verification report
32. Works should stop should contamination be identified.
33. Submission/approval of detail (including surfacing material) of x2 footpath links leading to and linking into Knutsford Footpath 1
34. Submission/approval of a scheme of improvement works to Knutsford Footpath 1
35. Submission/approval of an Affordable Housing Statement
36. Submission/approval of an Open Space Strategy
37. Submission/approval of a noise assessment (incl mitigation) in relation to adjacent playing fields and associated facilities
38. Submission/approval of a ball trajectory risk impact assessment (including mitigation) in relation to adjacent playing fields.
39. Submission/approval of an Active Environment Strategy
40. Any future development proposes a flat-roof design - Manchester Airport should be consulted.
41. Submission/approval of a SUDS scheme
42. Any exterior lighting should be capped at the horizon.
43. No solar thermal or solar PV equipment

Informatives

1. Developer to enter discussions with the Executive Director of Adults, Health, and Integration, or nominated colleague, to ensure that the Care Home provision proposal would be appropriate for Cheshire East Council in this location.
2. The inclusion of an on-site sports/play areas and allotment provision to be reviewed.

And a S106 Agreement to secure:

S106	Amount	Trigger
Highways – Contribute to the active travel scheme along the A50 King Edward Road and link into the new roundabout scheme being delivered at the Canute Place roundabout.	Commuted sum of £1.3 million pounds	Prior to occupation
Health – Commuted sum	Amount to be determined based on standard formula and by number of dwellings that ultimately gain approval. Index linked.	Prior to commencement
Education – Commuted sum	Amount to be determined based on standard formula and by number of dwellings that ultimately gain approval.	Prior to commencement
Affordable Housing - On-site provision	30% of total number of dwellings (rounded-up) shall be affordable.	In accordance with triggers detailed within required Affordable Housing Statement
Open Space & sport <ul style="list-style-type: none"> • Open Space – on-site provision • Open Space - Management • Commuted Sum – Allotments 	<p>Secure the required on-site open space requirements.</p> <p>Submission/approval of a Management and maintenance plan (incl off-site landscape buffers & footpaths)</p> <p>Establishment of a private management company</p> <p>Contribution towards off-site Allotments - £586.70 per family dwelling or £293.35 per apartment</p>	All - Prior to occupation

<ul style="list-style-type: none"> • Commuted Sum – Outdoor sport 	Submission/approval of Sports Needs Assessment to determine contribution amount and where it should be spent.	
<ul style="list-style-type: none"> • Commuted Sum – Indoor sport 	Amount to be determined based on standard formula and by number of dwellings that ultimately gain approval.	

Councillor G Marshall requested it to be recorded that he voted against the application.

In order to give proper effect to the Strategic Planning Board's intent and without changing the substance of its decision, authority is delegated to the Head of Planning in consultation with the Chair (or in their absence the Vice Chair) to correct any technical slip or omission in the resolution, before issue of the decision notice.

6 **23/0539N - LAND IN THE WESTERN PART OF BASFORD EAST, CREWE**

Consideration was given to the above planning application.

The following attended the meeting and spoke in relation to the application:

Ward Councillor: Councillor Steve Edgar, Councillor Alison Heler

Parish Councillor: Councillor John Cornell

Agent / Applicant: Ed Harvey

Councillor S Edgar vacated the meeting for the remainder of Item 6 following his statement.

RESOLVED:

For the reasons set out in the report, and the update report, the application be APPROVED as RECOMMENDED, subject to the following conditions:

FULL

1. Commencement of development (3 years)
2. Development in accordance with approved plans
3. Mix of employment uses (Uses Classes) and restriction on floorspace
4. Details of materials and finishes to be consulted with Liaison Group and Ward Councillor/s
5. Details of Hard Surfacing
6. Submission of details of landscaping scheme
7. Implementation of landscaping scheme
8. Submission of Landscape Management Plan
9. Details for provision of vertical standalone landscape feature alongside boundary with the West Coast Mainline

10. Details of Boundary treatments
11. Submission of Arboricultural Method Statement prior to commencement of development
12. Submission of a scheme for the protection of trees and hedgerows prior to commencement of development
13. Submission of a detailed service and foul and surface water drainage layout with regard to long term tree retention prior to the commencement of development
14. Submission of detailed design of outfalls to Basford Brook informed by the submitted illustrative proposals
15. Prior to the commencement of development, submission of a CEMP to include measures to safeguard the Mere Gutter and Basford Brook LWS throughout the construction phase and include details of the Ecological Clerk of works appointed to oversee its implementation.
16. Prior to commencement of development a detailed surface water drainage scheme to be submitted which includes a minimum three stage treatment for medium hazard surfaces and a minimum one treatment stage for low hazard surfaces in accordance with the submitted Sustainable Drainage Strategy Statement
17. Development to proceed in accordance with the submitted White Clawed Crayfish Mitigation Strategy prepared by TEP dated February 2024.
18. Submission and implementation of Amphibian Reasonable Avoidance Measures prior to the commencement of development to include staged vegetation clearance and measures to safeguard retained habitat during the construction process.
19. Reptile mitigation to be implemented in accordance paragraphs 7.66 – 7.68 of the submitted ES addendum dated February 2024.
20. Submission of proposals for ecological enhancement (bat and bird boxes etc.) prior to commencement.
21. Submission prior to commencement of amphibian friendly drainage systems and dropped kerbs to minimise risks to amphibians and other wildlife.
22. Safeguarding of nesting birds.
23. Entry into Natural England's District Level licencing scheme prior to commencement of works on site.
24. Submission of updated badger survey prior to commencement of development.
25. Detailed lighting scheme (bat friendly) to be submitted prior to installation
26. Submission and implementation of habitat creation method statement, and 30 year habitat management and monitoring strategy
27. Prior to commencement of development the detailed surface water drainage design and SuDS management plan shall be submitted and approved.
28. The development shall be carried out in accordance with the submitted flood risk assessment and mitigation measures detailed
29. Details of foul water drainage scheme
30. Provision of a ghost right turn lane on the existing spine road
Implementation of scheme for signalisation of the David Whitby Way/A500 roundabout
31. No occupation of Unit 2 (Phase 1) until access is available for use.
32. Prior to the occupation of each unit, an individual Travel Plan shall be submitted for that unit
33. Submission of a comprehensive travel plan framework for the site prior to first occupation.
34. Provision of Electric Vehicle infrastructure

35. Provision of Ultra Low Emission Boilers
36. Prior to commencement of development submission of Phase II ground investigation and risk assessment, and remediation strategy if necessary .
37. Contaminated land - Verification report
38. Contaminated land – soil testing
39. Measures to deal with unexpected contamination
40. Details of Levels
41. Public rights of way scheme of management
42. Programme of archaeological observation
43. Implementation of noise mitigation strategy
44. Submission of Construction Environmental Management Plan (residential amenity)
45. Community Liaison Group

OUTLINE

1. Outline Timescales
2. Outline Matters Reserved
3. Approved plans
4. Define mix of employment uses (Uses Classes) and restriction on floorspace – to include a minimum provision of 9,290 sqm of Class E(g)(i) office space
5. Phasing plan to be submitted with first reserved matters application
6. All reserved matters applications to comply with the Design Code and Regulating Plan
7. Implementation of landscaping scheme
8. Submission of Landscape Management Plan
9. Details of Boundary treatments submitted with all reserved matters applications
10. Details of levels to be submitted with all reserved matters applications
11. Arboricultural Method Statement to be submitted with all reserved matters applications
12. If as part of any reserved matters application outfalls to Basford Brook are required, a detailed design of outfalls informed by the submitted illustrative proposals are to be submitted.
13. Prior to the commencement of any phase of development, a CEMP to be submitted including measures to safeguard the Mere Gutter and Basford Brook LWS throughout the construction phase to include details of the Ecological Clerk of works appointed to oversee its implementation.
14. The surface water drainage (SUDS) scheme for any reserved matters phase of development discharging to Basford Brook to include a minimum three stage treatment for medium hazard surfaces and a minimum one treatment stage for low hazard surfaces in accordance with the submitted Sustainable Drainage Strategy Statement.
15. Prior to commencement of development of any phase of development a detailed SUDS management plan is to be submitted to and agreed by the LPA. The agreed management plan to be implemented in full for the lifetime of the development.
16. Any in-channel works which are required to connect drainage scheme to Basford Brook a white clawed crayfish mitigation strategy shall be submitted in support of that reserved matters application.
17. Any reserved matters application to be supported by Amphibian Reasonable Avoidance Measures to include staged vegetation clearance and measures to safeguard retained habitat during the construction process. The agreed strategy to be implemented in full.

18. Reptile mitigation to be implemented in accordance paragraphs 7.66 – 7.68 of the ES addendum submitted in support of the outline application dated February 2024.
19. Any reserved matters application to be supported by an updated barn owl survey and a mitigation strategy informed by the submitted Barn Owl Mitigation Strategy prepared by TEP dated July 2023.
20. Submission prior to commencement of any phase of amphibian friendly drainage systems and dropped kerbs to minimise risks to amphibians and other wildlife.
21. Submission of proposals for ecological enhancement (bat and bird boxes) for each phase.
22. Safeguarding of nesting birds.
23. Reserved matters application to provide a detailed design for the proposed reptile mitigation area as detailed in the outline application submissions, including fencing and access for maintenance.
24. Entry into Natural England's District Level licencing scheme prior to commencement of works on site.
25. Submission of updated badger survey in support of any future reserved matters application.
26. Detailed lighting scheme (bat friendly) to be submitted with any future reserved matters application.
27. Submission and implementation of habitat creation method statement, and 30 year habitat management and monitoring strategy to reflect the target habitat detailed in the submitted biodiversity Net Gain Report (TEP Version 2.0 dated May 2024) as relevant to the particular reserved matters application.
28. No development shall take place until a detailed drainage strategy/design plan for the site has been submitted to and approved
29. The development shall be carried out in accordance with the submitted flood risk assessment and mitigation measures detailed
30. Details of foul water drainage scheme
31. Prior to the occupation of each unit, an individual Travel Plan shall be submitted for that unit
32. Submission of a comprehensive travel plan framework for the site prior to first occupation.
33. Provision of Electric Vehicle infrastructure
34. Provision of Ultra Low Emission Boilers
35. Prior to commencement of development submission of Phase II ground investigation and risk assessment, and remediation strategy if necessary.
36. Contaminated land - Verification report
37. Contaminated land - soil testing
38. Measures to deal with unexpected contamination
39. Public rights of way scheme of management
40. Programme of archaeological observation
41. Implementation of noise mitigation strategy
42. Submission of Construction Environmental Management Plan (residential amenity)
43. Community Liaison Group

And the prior completion of a S106 Agreement to secure the following:

S106	AMOUNT/REQUIREMENT	TRIGGER
A500 and A5020 access improvement contribution	£2,450, 0000	<ul style="list-style-type: none"> - Payment of £250k on commencement of Phase 2 (Buildings 4 to 10). - On commencement, a payment per s.q, metre of plots of Phase 2 (Buildings 4 to 10) to a maximum cumulative value of £2.2M.
<u>BNG</u> Details of offsite biodiversity measures including the submission of a habitat creation method statement and 30 year monitoring and management plan, and biodiversity metric	Submission of BNG measures, habitat creation method statement and 30 year monitoring and management plan, and biodiversity metric.	<ul style="list-style-type: none"> - Submit details for approval prior to commencement. - Delivery of habitat creation in the next appropriate season following commencement

Councillor A Harrison requested it to be recorded that he voted against the application.

In the event of any changes being needed to the wording of the Committee's decision (such as to delete, vary or add conditions/informatives/planning obligations or reasons for approval/refusal) prior to the decision being issued, the Head of Planning has delegated authority to do so in consultation with the Chairman of the Strategic Planning Committee, provided that the changes do not exceed the substantive nature of the Committee's decision.

7 23/2419M - ADLINGTON BUSINESS PARK, LONDON ROAD, ADLINGTON, SK10 4NQ

Consideration was given to the above planning application.

The following attended the meeting and spoke in relation to the application:

Agent / Applicant: Beverley Moss

RESOLVED:

For the reasons set out in the report, the application be APPROVED as RECOMMENDED, subject to the following conditions:

1. Time (3 years)
2. In accordance with approved plans
3. Materials as per application
4. Noise Mitigation – Implementation
5. Submission/approval of a remediation strategy

6. Submission/approval of a contaminated land verification report
7. Works should stop should contamination be identified
8. Provision of a pedestrian refuge on London Road prior to occupation
9. Implementation of cycle parking
10. Submission/approval of details of staff facilities within each unit (e.g. changing rooms, lockers, showers etc)
11. Implementation of landscape details (including boundary treatment)
12. Submission/approval of existing and proposed ground spot levels and finished floor levels
13. Planting of off-site landscaping prior to occupation of hereby approved development
14. Tree and hedgerow protection – Implementation
15. Submission/approval of an Arboricultural Method Statement
16. Entry into Natural England's Great Crested Newt District Level Licencing scheme
17. Submission/approval of updated 'other' protected species survey
18. Nesting birds
19. RAMS implementation (Hares and Hedgehogs)
20. Submission/approval of a Landscape and Biodiversity Enhancement Management Plan for a 30-year period
21. Submission/approval of Ecological Enhancement Strategy
22. Submission/approval of evidence that any blockages, damage or collapses in the existing drainage network intended to be used by the drainage strategy have been repaired.
23. Depending on outcome of Condition 22 either a) Submission/approval of a revised Drainage Strategy/design should it be deemed necessary or b) implementation of current Drainage strategy/design (incl management & maintenance)
24. Energy and Sustainability Statement – Implementation
25. Submission/approval of a waste management and outdoor storage plan required.

And a S106 Agreement to secure:

S106	Amount	Trigger
Highways – Accessibility and capacity improvements on the local highway network	Contribution of £453,642 towards Poynton Relief Road	Prior to the occupation/use of the 5 th unit
Landscape – Off-site management & maintenance	Relating to planting beyond the site edged red as detailed on the submitted plans	Prior to occupation/use

In order to give proper effect to the Strategic Planning Board's intent and without changing the substance of its decision, authority is delegated to the Head of Planning in consultation with the Chair (or in their absence the Vice Chair) to

correct any technical slip or omission in the resolution, before issue of the decision notice.

The meeting commenced at 10.00 am and concluded at 1.41 pm

Councillor S Gardiner (Vice-Chair, in the Chair)

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Application No: 22/2476N

Location: Land Off Minshull New Road And, FLOWERS LANE, LEIGHTON

Proposal: Reserved Matters application for approval of appearance, landscaping, layout and scale following Outline approval 19/2178N for erection of 304 dwellings on Parcels A and B

Applicant: Bloor Homes

Expiry Date: 25-Oct-2022

SUMMARY

This application proposes the Reserved Matters for two of the parcels of land, approved in outline and subject to a Section 106 legal agreement, under application reference 19/2178N. It is one of four separate applications being considered on this agenda.

These two parcels (A & B) make up the southern most part of the site, with Bloor Homes being the applicant here. A number of changes have been made to the application since it was submitted, with the latest amendments being received at the time of writing this report.

Highways have raised no objections, subject to some points of detail being agreed by condition, and an informative. The PROW team have no objections and the proposed footpath/cycleway provision is considered to be good.

The majority of the Ecology and Landscaping issues identified in this report have now been resolved, and although there are some outstanding matters to do with tree impacts, it is anticipated most of these can be addressed and Members will be updated on this matter.

Extensive discussions have been had regarding urban design and the Council's Urban Design Officer now feels the proposals have improved significantly and are therefore supported in design terms.

The Council's Flood Risk (LLFA) team have similarly been involved in detailed discussions with the applicant and detailed replies to matters raised have been submitted by the applicant. The LLFA's latest comments were awaited at the time of writing this report, but the main issues have now appear to have been addressed, and if necessary outstanding matters can be conditioned.

There are no objections on the grounds of affordable housing, public open space, residential standards or environmental matters.

RECOMMENDATION

Approve with conditions

SITE DESCRIPTION

These two separate, but adjacent sites form parcels A & B of a greater site forming the southern part of the Leighton West development site which consists of 4 elements.

This element as described above, is split in two:

Parcel A: This most southerly of the sites, has frontages to the new North West Crewe Package road to the south, to a site which is currently vacant but has outline planning permission for housing (owned by CEC) to the east. The western and northern boundaries are to open land and proposed as various forms of open space as part of the green infrastructure land also on this agenda.

Parcel B: This site sits to the north east of Parcel A and has the new road to the west, and Minshull New Road to the east. The southern boundary is marked by a public footpath (Leighton FP6), whereas the northern boundary is formed by a hedge line, separating it from Parcels to the north, also being considered on this agenda.

There are hedgerows with some trees to the site boundaries, and some scattered trees with the sites themselves, however they are limited in number and the sites are generally open in character.

There are no conservation areas on or adjoining the site, and there are no listed buildings on or adjacent to the site.

Three public footpaths cross this site. Crewe FP20, that becomes Leighton FP6 runs from the new road south of the site and crosses part of the site running due north. Leighton FP3 runs east west forming the southern boundary of Parcel B as noted above, and the running north of Parcel A.

PROPOSAL

The application title reads:

“Reserved Matters application for approval of appearance, landscaping, layout and scale following Outline approval 19/2178N for erection of 304 dwellings on Parcels A and B.”

Reserved Matters approval is sought for Appearance, Landscaping, Layout & Scale.

The site makes up a south central part of LPS4, with no current proposals for the land to the south and east which also received outline planning permission under reference 16/2373N referred below. Land to the north is considered under separate applications on this agenda, both for further residential parcels and green infrastructure.

The main elements of the proposed development consist of:

Vehicular access to Parcel A is proposed from the new road to the east, with pedestrian access from 2 additional locations. Access to the adjacent site is also proposed for both vehicles and pedestrians, the latter linking into the public right of way which runs along the site boundary. Access to the area of proposed green infrastructure to the north and west is also proposed.

Vehicular access to Parcel B is proposed from the new road to the west, and a link is proposed to the parcel to the north. Pedestrian access is proposed from several locations including linking the PROW to the south.

304 dwellings (Use Class C3) broken down as follows:

- 273 Private dwellings a mix of 2, 3, and 4 beds
- 31 Affordable dwellings a mix of 1, 2, 3 and 4 beds

Number of Beds	Number of Units	%
Private Dwellings		
2	30	11%
3	127	47%
4	116	42%
Affordable Dwellings		
1	8	27%
2	14	47%
3	8	23%
4	1	3%

Finally, play provision is proposed within parcel B (a NEAP) and adjacent to the parcels as set out further in the report below, within the green infrastructure area.

An Environmental Impact Assessment (EIA) was submitted with the outline application.

RELEVANT PLANNING HISTORY

Outline planning permission was granted under:

19/2178N Outline planning approval for the development of up to 850 residential units (Use Class C3), land reserved for new primary school, a local centre (Use Class A1-A4, AA, B1a, C3 and D1) and associated infrastructure and open space. Land off Minshull New Road and FLOWERS LANE, LEIGHTON APPROVED Nov 2021

On the adjacent site to the south, not subject to this application:

19/1371N Outline application for the development of up to 400 residential units (Use Class C3) and associated infrastructure and open space - LAND OFF, MINSHULL NEW ROAD, LEIGHTON, CREWE APPROVED

These two sites made up a significant amount of the Local Plan allocation LPS 4.

In addition, planning approval was granted for a series of Highway works in the immediate vicinity:

18/6118N A proposed series of highway infrastructure measures and associated works, in the Leighton area of Crewe, and known as the North West Crewe Package - Land Between Flowers Lane Minshull New Road The A530 Middlewich Road And North Of, PYMS LANE, CREWE APPROVED March 2019

Finally, this is one of four reserved matters applications submitted for Leighton West. All are on this agenda. The others are:

22/2499N Reserved matters application for appearance, landscaping, layout and scale with respect to 304 dwellings on Parcels C, D, E, F and G. An Environmental Statement was submitted at the time of the outline application. Land Off Minshull New Road And, FLOWERS LANE, LEIGHTON

22/2500N Reserved matters application for appearance, landscaping, layout and scale with respect to 200 dwellings on Parcel H. An Environmental Statement was submitted at the time of the outline application. Land Off Minshull New Road And, FLOWERS LANE, LEIGHTON

22/3228N Reserved matters application for appearance, landscaping, layout and scale with respect to green infrastructure and open space across the outline application area - approved outline application 19/2178N. Land Off Minshull New Road And, FLOWERS LANE, LEIGHTON

Other approvals under LPS 5, to the north include:

16/2373N Outline application for the construction of up to 400 dwellings with garaging; parking; public open space; landscaping; new vehicle and pedestrian accesses; highway works, foul and surface water drainage infrastructure and all ancillary works. - Land At, Flowers Lane, Leighton APPROVED January 2018

20/3210N Reserved matters approval sought for access, appearance, landscaping, layout and scale. following outline permission for the construction of up to 400 dwellings with garaging; parking; public open space; landscaping; new vehicle and pedestrian accesses; highway works, foul and surface water drainage infrastructure and all ancillary works. Land At, FLOWERS LANE, LEIGHTON APPROVED July 2021

This development is now on site.

Finally, there are numerous approvals at Leighton Hospital, the most relevant recent approval being a car park on the north eastern side fronting Flowers Lane.

POLICIES

Cheshire East Local Plan Strategy (LPS) – 2010-2030

PG1 – Development Strategy
PG6 – Open Countryside
SD1 - Sustainable Development in Cheshire East
SD2 - Sustainable Development Principles
IN1 – Infrastructure
IN2 – Developer Contributions
SC1 – Leisure and recreation
Sc2 – Indoor and outdoor recreation
SE 1 - Design
SE 2 - Efficient Use of Land
SE3 – Biodiversity and Geodiversity
SE 4 - The Landscape
SE 5 - Trees, Hedgerows and Woodland
SE 6 - Green Infrastructure
SE 13 - Flood Risk and Water Management
CO1 – Sustainable Travel and Transportation

LPS4 – Leighton West
LPS5 – Leighton

Site Allocations and Development Policies Document (SADPD) 2022

GEN 1 - Design principles
GEN 4 - Recovery of forward-funded infrastructure costs
ENV 1 - Ecological network
ENV 2 - Ecological implementation
ENV 5 – Landscaping
ENV 6 - Trees, hedgerows and woodland implementation
ENV 7 - Climate change
ENV 14 - Light pollution
ENV 16 - Surface water management and flood risk
HOU 1 - Housing mix
HOU 8 - Space, accessibility and wheelchair housing standards
HOU 13 - Residential standards
HOU 14 - Housing density
HOU 15 - Housing delivery
INF 1 - Cycleways, bridleways and footpaths
INF 3 - Highway safety and access
INF 6 - Protection of existing and proposed infrastructure
INF 9 – Utilities
REC 3 - Open space implementation
REC 5 - Community facilities

Neighbourhood Plans:

Crewe has not made any progress towards making a Neighbourhood Plan, and Minshull Vernon is at Regulation 7 Stage: Designated Neighbourhood Area, but at this stage can be given very little weight.

Other Material Considerations

National Planning Policy Framework

CONSULTATIONS (External to Planning)

United Utilities: Whilst originally objecting to the application, they have now confirmed the applicant's more recent submissions have addressed their concerns and they raise no objections subject to informatives concerning asset protection.

SP Energy Networks: No objections are raised, but they remind the applicant of their health & safety responsibilities around the significant electricity infrastructure which passes through the site, and the safe working practices that should be employed.

CEC Head of Strategic Infrastructure: No objections subject to a condition and an informative.

CEC Housing: Whilst originally raising objections to the application, as there was uncertainty about the affordable numbers and mix, following the submission of additional supporting information Housing have now confirmed they have no objections to the application.

CEC Public Rights of Way: In their original comments they raised a number of issues with the proposals, however following submission of revised plans these issues have been addressed and they now raise no objections.

CEC Environmental Health: No objections are made, subject to reminding the applicant of their responsibilities to discharge the conditions under the outline approval relating to amenity and air quality, and the contaminated land team recommends a further condition. Informatives are recommended.

CEC Flood Risk Manager: Extensive discussions have taken place and updated comments are awaited at the time of writing the report.

ANSA: Now that the range of facilities has been clarified across the site they raise no objections, however they recommend the detailed design of facilities is conditioned.

Archaeology: No comments have been received in relation to this application, however it is suspected their comments on the adjacent sites to the north would equally reply and as such an archaeological condition would be required.

VIEWS OF THE TOWN/PARISH COUNCILS

Minshull Vernon and District Parish Council: No comments received

Crewe Town Council: "The committee objects on the following grounds:

- i. That the site does not provide for adequate amenity and play facilities for the scale of development
- ii. That the scale of the development will present an unsustainable burden on social infrastructure (e.g. schools, health)
- iii. That net biodiversity gain has not been established or evidenced, which therefore does not meet Cheshire East Planning policy”

OTHER REPRESENTATIONS

Two residents have written in objecting to the application, one as follows:

“The houses planned will result in unnecessary noise, disruption and dust without compensating neighbours. Amenities will not be built until houses are all built. There is no clear cycle routes planned or proposal for sustainable heating or electric car charging. These should be prioritised to ensure electricity capacity is available for all.”

OFFICER APPRAISAL

Principal of Development

The site is allocated in Local Plan policy LPS 4 and has the benefit of outline planning permission as set out above. The outline set out the requirements for the site, and the Section 106 contributions/requirements. The policy reads:

LPS 4 Leighton West, Crewe

Development at Leighton West will result in the creation of a new sustainable neighbourhood to support Leighton Hospital and provide a key site for the development of an automotive research, development and supply hub, in partnership with Bentley Motors which is located in very close proximity to the site. The development of Leighton West over the Local Plan Strategy period will be achieved through:

1. Contributions to health infrastructure and the provision of land adjacent to Leighton Hospital, for its future expansion, to ensure that the future health care needs of the area can be met;
2. The delivery of around 850 new homes (at a variety of densities). The design, density and scale of the development should reflect the fact that the site lies in a transitional location between the higher density urban area and the rural area. The surrounding development is predominantly suburban and the development of the site should reflect this. The development of the site will be masterplan-led, including a design code, which will consider its location, constraints and opportunities;
3. The inclusion of key worker housing, for the employees of Leighton Hospital;
4. The delivery of a new mixed-use local centre that will serve Leighton Hospital and nearby residents including:
 - i. Provision of retail appropriate to meet local needs;
 - ii. Community facilities;
 - iii. Public House;
 - iv. Children's day nursery;
 - v. A new primary school; and
 - vi. Leisure facilities;
5. Around 5 ha of additional employment land located at the southern end of the site including a science/energy park which could include advanced/automotive engineering and manufacturing;
6. The incorporation of green infrastructure, including:

- i. Green corridor;
- ii. Allotments;
- iii. Open space including formal sports pitches, multi use games area, outdoor gym, equipped children's play space and facilities for teenagers;
- 7. A new bus interchange for the hospital and nearby residential areas;
- 8. On land to the north of Pym's Lane, there is potential for a science/energy park and delivery of a key site for the development of an automotive research, development and supply hub, working in partnership with Bentley Motors. This area also has the potential to include a geothermal plant and district heating hub;
- 9. The widening and/or realignment of Smithy Lane, to provide access to the site and improved access to Leighton Hospital for emergency vehicles and suitable footpath and cycle lanes;
- 10. A package of network improvements to provide improved access to Leighton Hospital and surrounding residential development including emergency vehicles and suitable footpath and cycle lanes; and
- 11. An improved 'emergency portal' for blue light vehicles being able to access the hospital's Accident and Emergency Department from Smithy Lane.

As the application proposes the erection of dwellings which in principle accord with the Local Plan allocation and the parameters set in the outline approval, it is considered the application is in principle acceptable. The (relevant) site specific requirements are explored below.

Highway Implications

This site forms part of the local plan allocation LPS4 and already has outline approval where access was determined including the wider highways impact and any mitigation required. This current proposal is the reserved matters for the parcels of land to the southern end of the wider site.

The various parcels of land will be accessed off the new highway infrastructure approved in a previous application and the street hierarchy within the site reflects CEC guidance and adoptable requirements, other than the small shared private drives at the end of cul-de-sacs. The internal junctions will have adequate visibility and parking provision will be to CEC standards. A small number of driveways are shorter than required and will need lengthened slightly to ensure parked cars do not overhang onto the highway; this is a minor amendment that can be conditioned.

There will be sustainable connections to the surrounding footpath network and also the main cycle route that will run through the wider site, and also connections to Minshall New Rd.

No objection is raised subject to a condition as discussed above, and an informative.

Public Rights of Way/Cycle routes:

Commenting on the original proposals, the PROW Team state that the application documents depicted the Public Rights of Way mostly running along specific pathways. The section of Leighton FP 3 that runs along the southern edge of Phase B site is accommodated within an identified pathway however there is no detail of the width, surfacing, gradients, surrounding landscape and road crossing detail where the path crosses the new road. The continuation of the section of Leighton 3 that runs into the northern edge of Phase A is also accommodated along a pathway but again there were no details of the items mentioned above.

Where Leighton FP 6 entered the Phase B site halfway down on the western edge, the path was obstructed by shared drives, a street and one house. There was no provision for the footpath or any indication that the applicant intends to apply to divert this section of footpath.

Following submission of revised plans these concerns have been addressed and the PROW Team now raise no objections to the application.

In terms of cycle access there are two main elements of provision. Firstly, there is the cycle route which runs alongside the newly aligned Flowers Lane with linkages to the Hospital and Crewe Town Centre.

Re-enforcing these highway links are those now proposed through this application including the main footpath/cycle ways linking to the area of green infrastructure to the north and adjacent housing site.

The overall provision is considered to acceptable and should encourage residents to walk and cycle. In line with policy objectives.

Landscape

The Council's Landscape Architect has made the following general observations:

Parcel A is generally acceptable. There are a few tight spots where the streets have little tree planting, and the applicant should be encouraged to consider a few more medium sized street trees.

The interface with the linear park could be better integrated into cohesive footpath network, as discussed with the applicant, and hopefully will be picked up in a composite masterplan for this area.

Parcel B is generally acceptable. There are a few tight spots where the streets have little tree planting. The LEAP area has too much wildflower and little amenity grass. There are 'bald spots' within the street tree planting which is a touch concerning.

The applicant has looked at these matters and has now submitted as Landscape Masterplan to better set the context. More tree planting is now proposed in the housing areas, addressing the main comment.

Trees

An amended Arboricultural Impact Assessment (AIA) and an updated Arboricultural Method Statement (AMS) has been submitted further to earlier forestry comments. Clarification has been provided regards several of the issues raised as requiring more detail and explanation.

It is understood that tree and hedgerow losses which have occurred since the preliminary survey was carried out at outline stage have arisen because of Highway Infrastructure projects and the supporting plans are now annotated to confirm where this is believed to be the case.

The updated AIA and AMS has now been broken down by the defined parcels of development and makes provision to show all trees including those presently shown for removal shown by tree quality category colour with Root Protection Area's (RPA's), so that impacts can be better understood. Some explanatory notes have been provided regarding the pre-agreed loss of TPO'd trees and those trees already removed for the highway's infrastructure projects.

The AIA indicates that of the trees surveyed across the wider site that 3 individual moderate quality B Category trees, 1 individual and 1 group of low-quality C Category trees, and 1 poor quality U Category tree as well as a combined total of 88.7 linear metres from 4 hedgerows are proposed for removal to accommodate the proposals associated within Parcels A and B of the development.

The submitted Assessment of Important Hedgerows considers the whole of the site and has not been broken down to impacts on the submitted applications. Twelve hedgerows (H1-H12) have been identified as important in accordance with criteria 5a of the Hedgerow Regulations.

5. The hedgerow:

a) is recorded in a document held at a Record Office as an integral part of a field system pre-dating the Inclosure Acts

Hedgerow 1 is also importance in accordance criteria 1.

1. Any hedgerow that marks the boundary or part of the boundary, of at least one historic parish or township; and for this purpose, "historic" means existing before 1850.

The assessment confirms that sections of hedgerow H2, H5, H6, H7 AND H8B0 are to be removed as part of proposed development for Northwest Crewe Package and includes the removal of sections for Highway works. Additional removal of H3 is required for a bus route and access. The overall development will arise in the partial loss of hedgerows which form a 19th Century field system but most of the hedgerows are shown for retention where possible and the report has concluded that the overall harm is; 'less than substantial'. Mitigation is proposed of 2,300m of new hedgerows. As hedgerows have been found to be 'Important' under the above referenced criteria of the Regulations, the loss of the hedgerows to accommodate the development is a material consideration in the determination of the application although it appears that sufficient compensatory planting is being offered across the whole development area.

Having appraised the amended detail and proposals identified it is no clearer as to what the rationale is behind the loss of some moderate quality B Category trees within Parcels A and B other than to facilitate development and the proposed layout.

Initial observations are as follows; the relationship between high quality A Category tree (T167) and plots 363 and 364 is poor with just 12 metres separation between the stem centre of the mature Oak and the rear elevation of the proposed dwelling with the canopy of the tree dominating the available outside garden space.

Further detail should be provided as to why tree T99 could not be retained as it would be useful to understand why minor reconfiguration of footpaths and engineer designed solutions are not possible to facilitate the retention of this tree. The updated AMS does show all trees according to their surveyed quality category and some projected RPAs although these are not present for trees shown for removal.

Moderate quality B Category Oak (T138) is sited within the area of open space shown to accommodate the LEAP. The tree is described within the survey as 'a huge tree', and while not without some described evidence of decay and dead wood not unexpected for a tree of its proportions, it's described as very prominent. The suitability of an area for play in such proximity to one of the most significant remaining trees on the site is questionable and to minimise impacts arising from disturbance within the RPA and alleviate pressures for unnecessary pruning works, re location or reorientation of the LEAP should be considered. It is also noted that the new surfacing covering existing unsurfaced ground within the RPA will equate to slightly more than the 20% recommended in BS5837:2012.

The AMS as submitted does include a tree protection plan, makes provision for a methodology for working in relation to retained trees, and identifies areas where special measures would be implemented in respect of minor root pruning and construction of engineer designed surfaces. Notwithstanding this, considering the tree losses which have already been carried out because of the Highway improvements and further losses proposed arising from this layout, opportunities exist to retain further trees. Separation between tree T167 and the closest plot should be increased and minor amendments to the landscape layout considered in respect of pedestrian pathways and trees shown for removal to see if additional moderate quality trees such as T99 could be retained and to provide more confidence that the most significant trees on the site will have a sustainable relationship with residential property in the longer term.

The applicant has been looking at the impacts on trees identified above as part of the revised plans, as whilst the Council's Tree Officer has not formally objected to the application on the basis of tree impacts, at least some of these impacts would seem to be avoidable. At the time of writing this report the applicant was discussing amendments with the Council's Tree Officer which would allow for the retention/better protection of more trees, and Members will be updated on this matter in an Update Report.

Ecology

A number of conditions were attached to the outline consent at this site.

Condition 9 Ecological Monitoring Strategy:

Monitoring proposals are required by this condition are included in the submitted Habitat Management Plan TEP Version 3.0 dated 10.4.24.

Condition 11 Detailed design of attenuation ponds:

No detailed designs of attenuation ponds have been submitted with this reserved matters application. The applicant has confirmed that no attenuation ponds are proposed as part of this application.

Condition 12 Updated hedgerow impact assessment, mitigation and compensation strategy:

The submitted biodiversity assessment (January 2024) however advises that 2.6km of hedgerow would be lost across the entire outline site under the various reserved matters applications, with 2.17km of new hedgerow planting being provided. The originally submitted BNG metric concluded that the reserved matters applications would result in a net loss of 10.41% in respect of hedgerow biodiversity.

It was advised that the development proposals must ensure that the extent of hedgerow retained as part of the development is maximised and that any unavoidable losses of hedgerow are adequately compensated for by appropriate native species hedgerow planting. A greater length of replacement planting to that lost will be required to fully address the loss of biodiversity from hedgerow removal. Ornamental hedgerow planning is not acceptable as compensation for the loss of native species hedgerow.

Following the submission of further information to clarify the impacts of this development as opposed to losses for the road scheme – which appears to have caused the confusion, the Council's Ecologist comments:

"The proposed reserved matters application combined will result in the loss of 0.38km of hedgerow, including lengths of hedgerow considered to be 'Important' under the Hedgerow Regulation. The BNG

metric has been undertaken on the basis of 2.17km of new hedgerow planting being provided on site. This is based on PGLA Landscape Masterplan Rev 11.

The proposed hedgerow planting would result in a net gain of hedgerows of 30.57%. Therefore, if the loss of the existing hedgerows is considered unavoidable sufficient planting would be provided to compensate for that lost. This would however be dependent upon the reserved matters application being brought forward in accordance with the landscape master plan."

This masterplan would, should planning permission be granted, form one of the approved plans.

Condition 25 Updated badger and barn owl survey and mitigation strategy:

Satisfactory Badger and barn owl reports have now been submitted as required by this condition.

Condition 26 Details of any proposed lighting. The lighting strategy should reflect the Bat Conservation Trust Guidance Note:

Lighting plans have been submitted in support of this application. No information, such as the 1 lux lighting contour, as required by this condition has however been submitted. It is advised that the submitted lighting plan must be revised to include the 1 lux lighting contour and the lighting scheme must seek to avoid any light spill of greater than 1 lux on any suitable bat foraging or commuting habitat, such as boundary hedgerows or trees.

Condition 27 25 year habitat management plan:

An acceptable Management plan has been submitted.

Condition 28 The first reserved matters application shall be supported by a strategy for the incorporation of features to enhance the biodiversity value of the proposed development:

A Biodiversity Impact Assessment (TEP June 2022) which covers the full extent of the outline planning consent subject to the various current reserved matters applications was previously submitted in support of this reserved matters application. A revised BNG metric dated 22/1/24 has also now been submitted. The metric includes measurements for the areas of various habitats to be provided on site. The area of species rich grassland provide don site is less than that required by condition 28 as is the extent of hedgerow planting. The area of cornfield annuals and wet grassland is however greater than required by condition 28.

The assessment concludes that the proposed development (based on all of the reserved matters applications) would deliver a net gain for area-based habitats of 12%, but originally a net loss of -10.41% of hedgerow biodiversity. This has subsequently been amended as discussed above.

Proposals for the provision of bird and bat boxes and gaps in garden fences have been submitted as required by this condition. It must however be ensured that bat/bird boxes are proposed to be attached to 30% of consented dwellings in accordance with the Cheshire East Design Guide.

Detailed method statements for the creation of species rich grassland and cornfield annual meadow habitats are outstanding. These must be submitted prior to the discharge of this condition.

If reserved matters consent is granted an additional condition would be required to safeguard nesting Birds.

Whilst ideally all matters would have been resolved now, outstanding matters relating to the discharge of conditions highlighted above, will be picked up in their subsequent discharge.

Urban design

The development has been assessed as a whole (considering the four Reserved Matters (RM) applications collectively) using the Building for a Healthy Life (BHL) considerations. Performance is summarised in the table below. A more detailed commentary in relation to each of the considerations has also been provided, but not included within this report.

Integrating into the Neighbourhood				Distinctive Places				Streets for All			
1 Natural Connections	2 Walking, cycling public transport	3 Facilities and services	4 Homes for everyone	5 Making most of what's there	6 Memorable character	7 Well defined streets/spaces	8 Easy to find your way around	9 Healthy streets	10 Cycle and car parking	11 Green and blue infrastructure	12 Back of pavement, front of home
G	G	G	G	G	A	G	G	A	A	A	A

Summary/conclusions

This is a large and complex scheme compounded by the submission of several separate ARM applications, prepared jointly by the two developers. Significant input has been necessary to improve the quality of the originally submitted proposals. Key aspects are:

- Securing a more responsive, forward-thinking approach to the design of the local centre and other parcels within proximity to Leighton Hospital.
- Ensuring the residential component of the local centre acts as a positive lead for the design and development of the future commercial area including securing detailed coding to help guide the design of the commercial area and associated public realm.
- Developing a character area approach for the remainder of the scheme to ensure a degree of continuity and consistency between developers but also some localised individuality (albeit this could have been taken a little further).
- Ensuring there is a clear street hierarchy, with tree lined avenues within the larger parcels and stronger, more formal gateways into smaller ones, with a supplementary hierarchy of streets and spaces, appropriately detailed and surfaced to suit their function.
- Ensuring appropriate levels of landscaping along the edges of development parcels.
- Inclusion of feature spaces within layouts to help reinforce the social function of streets, better handle concentrations of parking and to create points of incident and interest.
- Seeking to integrate existing landscape features within the layout whilst accepting the constraints imposed in certain locations and the need to connect to adjoining developments and the approved highway improvement scheme (Northwest Crewe package currently under construction).
- Ensuring the inclusion of blue infrastructure as a positive element of the place creation adhering to principles within the recently adopted SuDS Design Guide SPD, with a series of basins, swales and ditches for the outlying development areas and within the main central area of POS, and a more urban approach for the local centre, including living surfaces to buildings and structures, rain gardens and channels/rills within streets and spaces.
- Improved connectivity within the development and to the surrounding area with integrated public rights of way, combined footpath/cycleway routes and stronger interrelationship and connectivity to, within and along the central main POS area defined by the pylon route.
- Enhancements to the strategic approach to landscape design, including reflecting and building upon the character of the spaces, integrating existing landscape features, provision of varied play and leisure opportunity and inclusion of local food growing, including a central allotment area, orchard and on the doorstep, more informal, food growing within and adjacent to housing.

Whilst in certain respects the design response could have gone further/been more creative, the work invested has enhanced the scheme considerably from the initial submissions and will lead to a better quality and more cohesive development that responds more favourably to policy LPS4 (Parcels A-G) and LPS5 (Parcel H) of the CELPS, local design policy and guidance and the ambitions of the spatial design code submitted with 19/2178N. The proposals are therefore supported in design terms.

Residential standards

As the proposed development parcels A & B are well separated from adjacent housing areas on Minshull New Road, and to a farm complex to the west there are no issues with separation distances in this regard.

Within the development parcels themselves the majority of the properties meet or exceed the required separation distances, and in the few cases where they don't the properties are slightly off set from one another or there are good urban design reasons for it.

Environmental Impacts

Environmental Protection have confirmed they have no objections to the application with regards to general amenity (including noise) and air quality subject to satisfactory discharge of the relevant conditions on the outline and informatives. The contaminated land team comment that they have no objection to the application subject to the following comments with regard to contaminated land:

- RoC Consulting Phase 1 Desktop Study was submitted in support of the outline application.
- The report identifies a number of potential pollutant linkages that require further investigation. The report recommends that a Phase II investigation is conducted at the site.
- A rigorous monitoring-based ground gas risk assessment will be required to assess the potential impact of ground-gas migration from the landfills adjacent south.

As such, and in accordance with the NPPF, the Contaminated Team recommends that that conditions, reasons and notes be attached should planning permission be granted.

Flood Risk/Drainage

Extensive discussions have taken place with the applicant's drainage engineers, with matters of relating to drainage calculations, finished floor levels, management arrangements and details of a pumping station being discussed. The applicant has submitted detailed replies to all matters raised, but at the time of writing this report the Flood Risk Manager's updated comments are awaited. It is considered that if there are any outstanding matters raised, they could be conditioned.

Public Open space

The public open space provision was set out at the outline stage and captured by the Section 106 Agreement. Overall, the site includes the following facilities:

- Multi Use Games Area (MUGA)

- Teen Shelter & Play
- Neighbourhood Equipped Area of Play (NEAP)
- An orchard
- Allotments
- Local Equipped Areas of Play (LEAP)
- Local Areas of Play (LAP) - 6 No.
- Areas of outdoor gym equipment
- Areas of incidental open space

Whilst many of these facilities are included with the common areas of green infrastructure which runs through the centre of the site, and along the northern boundary some are located within individual parcels where it was deemed preferable to give easier access for residents.

For this application a NEAP is proposed in Parcel B, and 3 LAPS, together with 2 areas of outdoor gym equipment in the green infrastructure area close to the parcels.

Due to the way the information was presented, ANSA where initially unclear what provision was being made, however following the submission of a Landscape Masterplan which sets it out more clearly they have confirmed they have no objections to the provision as submitted. There are however detailed design issues with the individual areas of provision, so ANSA have asked for the detailed design of each of the items listed above (incidental open space excluded) to be subject of a condition.

Affordable Housing

The Section 106 Agreement attached to the outline approval for the site required 10% of the houses on site to be affordable – based on a viability appraisal which was independently assessed at the time. The policy compliant 35% Shared Ownership/65% Affordable Rent was also required.

Now that the applicant has clarified the provision, by the submission of an updated Affordable Housing Statement which shows the number of units in each tenure, the number of beds and room sizes, and how they are laid out on site Housing have confirmed they are happy with the proposals. In parcels A & B 31 units are shown 10% of the 304 units proposed. All the properties meet the NDSS.

It is now considered that the affordable housing provision is acceptable.

Other matters

Crewe Town Council and a resident have expressed concern about the lack of facilities in the area. Although this was addressed at the outline stage of the application(s) the local plan allocation (as set out above in the policy section), sets out what is required, namely a “new mixed-use local centre”. These reserved matters applications only seek permission for residential development – with associated POS/Landscaping/Green space, but clearly show the school site adjacent to parcel H on the most northerly site, and the local centre site adjacent to the southern side of the hospital adjacent to parcel F.

Residents have raised the issue of vehicle charging facilities, and whilst this is an important part of any development, it was a matter that was conditioned at the outline stage and as such this condition will need to be discharged. Members will also be aware it is now a requirement of Building Regulations.

CONCLUSIONS

This application proposes the Reserved Matters for two of the parcels of land, approved in outline and subject to a Section 106 legal agreement, under application reference 19/2178N. It is one of four separate applications being considered on this agenda.

These two parcels (A & B) make up the southern most part of the site, with Bloor Homes being the applicant here. A number of changes have been made to the application since it was submitted, with the latest amendments being received at the time of writing this report.

Highways have raised no objections, subject to some points of detail being agreed by condition, and an informative. The PROW team have no objections and the proposed footpath/cycleway provision is considered to be good.

The majority of the Ecology and Landscaping issues identified in this report have now been resolved, and although there are some outstanding matters to do with tree impacts, it is anticipated most of these can be addressed and Members will be updated on this matter.

Extensive discussions have been had regarding urban design and the Council's Urban Design Officer now feels the proposals have improved significantly and are therefore supported in design terms.

The Council's Flood Risk (LLFA) team have similarly been involved in detailed discussions with the applicant and detailed replies to matters raised have been submitted by the applicant. The LLFA's latest comments were awaited at the time of writing this report, but the main issues have now appear to have been addressed, and if necessary outstanding matters can be conditioned.

There are no objections on the grounds of affordable housing, public open space, residential standards or environmental matters.

RECOMMENDATION

Approve subject to the following conditions;

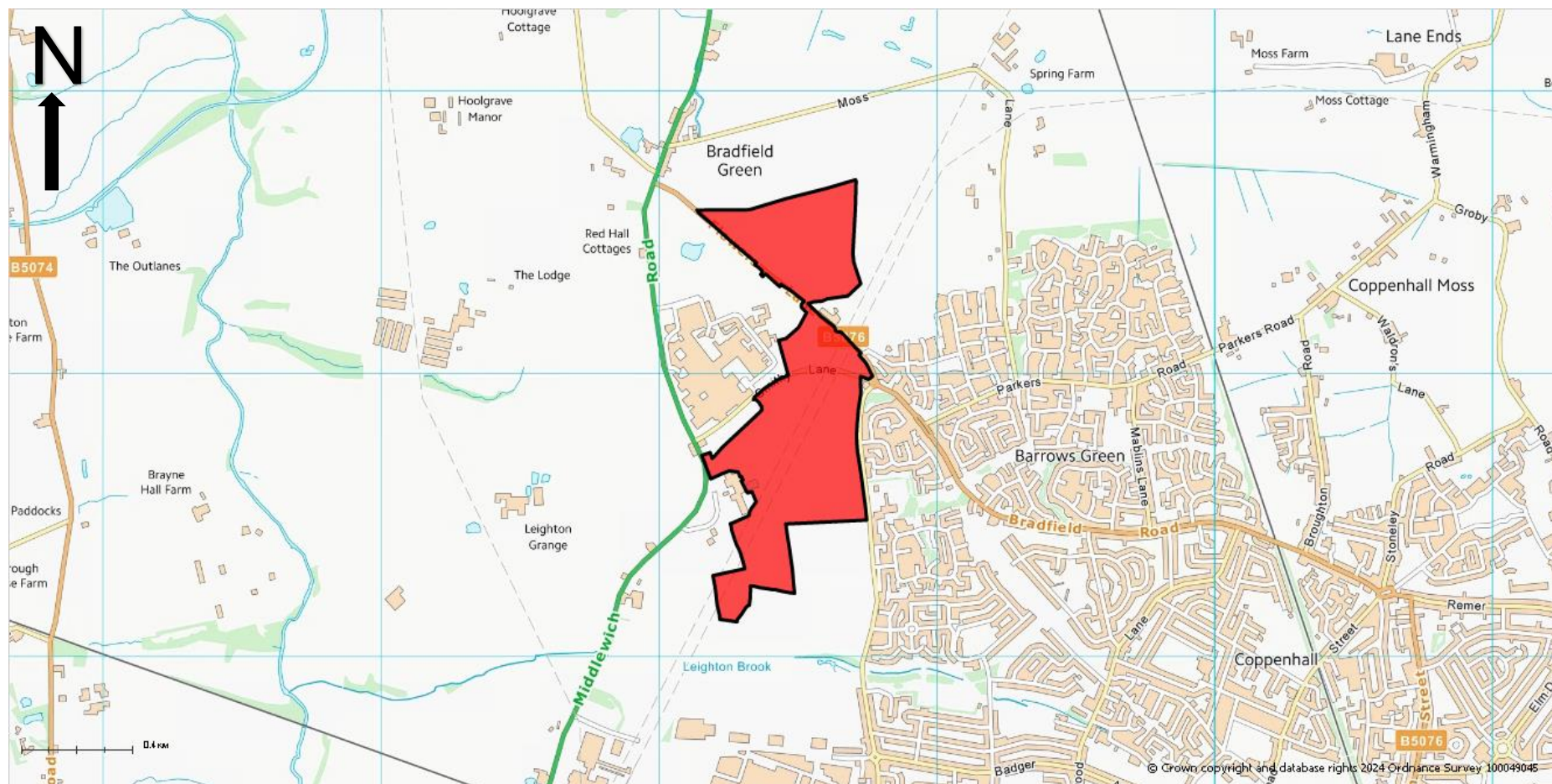
1. Approved plans
2. Bird nesting
3. Submission of a Phase II ground investigation and risk assessment
4. Submission of a Verification Report
5. Soils testing
6. Unexpected contamination
7. Archaeological mitigation
8. Detailed design of the individual elements of the play areas and the outdoor gyms to be agreed.
9. Detailed landscaping to be submitted and approved.
10. Prior to commencement plans of the driveways for plots 303, 330-331, and 365 should be submitted and approved.

Informatives;

- SP Energy informatives
- Public Rights of Way
- Environment Protection Informatives

- Highways: The applicant will be required to enter into a s38 Agreement regarding the construction and future adoption of the internal road layout.

In the event of any changes being needed to the wording of the Committee's decision (such as to delete, vary or add conditions/informatives/planning obligations or reasons for approval/refusal) prior to the decision being issued, the Head of Planning has delegated authority to do so in consultation with the Chairman of the Strategic Planning Board, provided that the changes do not exceed the substantive nature of the Committee's decision.



22/2476N

Land Off Minshull New Road
and Flowers Lane

LEIGHTON

Revision.		
A	Boundary updated to match latest planning layout	01.11.23 AF

CREWE
Leighton West

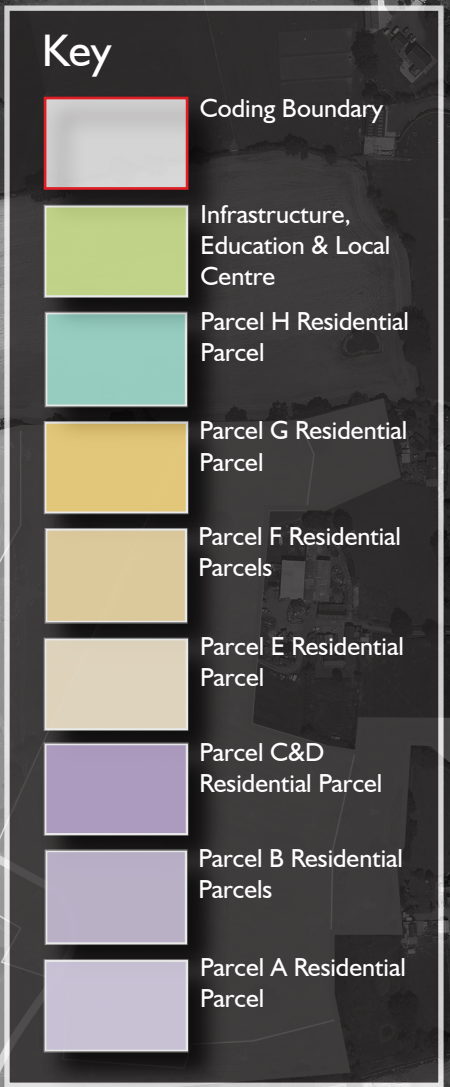
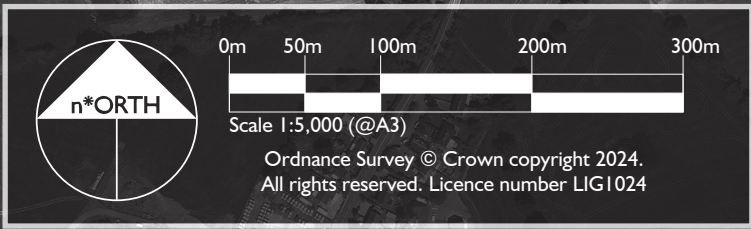
**Location Plan
Phase A + B**

DATE: **16.03.2022**
SCALE: **1:2500 @ A1**
DRAWN: **AF**
CHECKED: **RTD**

BLOOR HOMES®

Drawing No. NW112-PD-1002 A

Location Plan - Parcel A + B



e*SCAPE 15 years
urbanists 2009 - 2024

Project Title
Leighton West, Crewe

e*SCAPE Job No.
021-025

Client
Bloor/Countryside

Drawing Number
021-025-P007

Revision
REV A

Drawing Title
Parcel & Phase Boundaries

Scale
1:5,000 @ A3

Date
May '24



CREWE
Leighton West

Site Layout
Phase B

DATE: 14.03.2022
SCALE: 1:500 @ A0
DRAWN: AF
CHECKED: RTD

BLOOR HOMES

Drawing No: NW112-PD-2001 G

Legend:

- Denotes dwellings connected with a carport
- Denotes Existing Trees (RPA) / Vegetation
- Denotes existing trees / vegetation to be removed / thinned out
- Proposed estate trees (indicative only, please refer to site landscaping)
- Proposed Jacobs trees / vegetation
- Denotes footpath (Solid line)
- Denotes grassed service verge (Dashed line)
- Denotes Sub-Station
- Affordable Rent
- Shared Ownership

Table 1: Affordable Rent

Type	Beds	Stores	No
Dekker	2	2	12
Dekker MT	2	2	3
Buxton	3	2	14
Lyford	3	2	11
Lyford SD	3	2	4
Kane	3	2	10
Huxley	3	2	14
Whalton	3	2	5
Forbes	3	2.5	20
Locke	4	2	14
Whetton (QA+CL)	4	2	1
Watercroft (QA+CL)	4	2	16
Lullingstone (QA+CL)	4	2	19
Wallingford	4	2	9

Table 2: Shared

Type	Beds	Stores	No
NSS-2B3P Silkinn	2	2	4
NSS-3B4P Silkinn	3	2	2

Table 3: Affordable Rent

Type	Beds	Stores	No
F01 Sahlgberg GF-M4(3)	1	1	1
1B01 Sahlgberg FF	1	1	1
1B2P Tiverton-M4(3)	1	1	2
NSS-2B3P Silkinn	2	2	2

Table 4: Shared

Type	Beds	Stores	No
NSS-2B3P Silkinn	2	2	4
NSS-3B4P Silkinn	3	2	2

Table 5: Total

Type	Beds	Stores	No
Total			164

Revisions:

Revision	Description	Date	By
G	Layout amended to suit planning comments / Extra footpath link added to phase B / Paths amended to go around allocation ponds / Paths amended around LEAP / Boundary fence line to corner plots amended in line with comments / More street food planters added / Shared drive in front of 112-113 adopted	18.06.24	AF
F	Revision omitted	18.06.24	AF
E	Affordable tenure updated (layout/schedule)	15.01.24	AF
D	Layout updated to match planning comments	13.09.23	AF
C	All in Model Space plots updated to issue 6.1 Working Drawings / Accommodation Schedule updated accordingly	25.07.23	TJW
B	Layout updated following comments from planners / Bunglows have been added / Drives and houseplots updated to match M4(3) requirements of 6% / Drives updated to match M4(2) requirements of 30%	25.05.23	AF
A	Updated following planners comments	15.03.23	AF

Leighton West, Crewe - Phase B



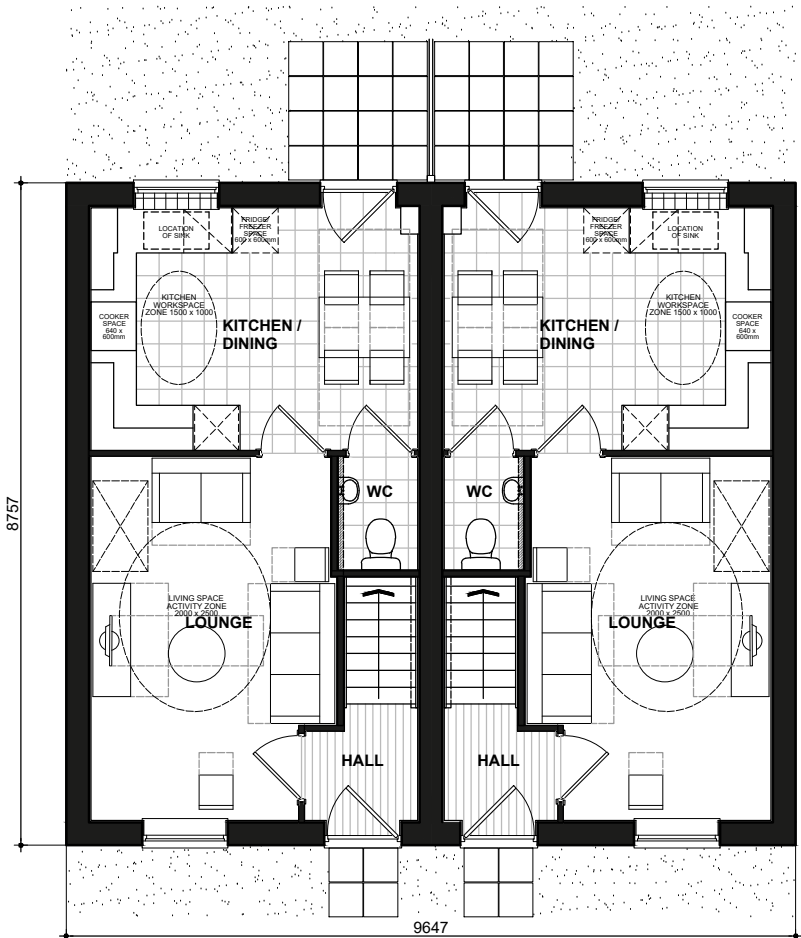
QANSS.2B3P
FRONT ELEVATION

QANSS.2B3P-1

QANSS.2B3P-1
SIDE ELEVATION

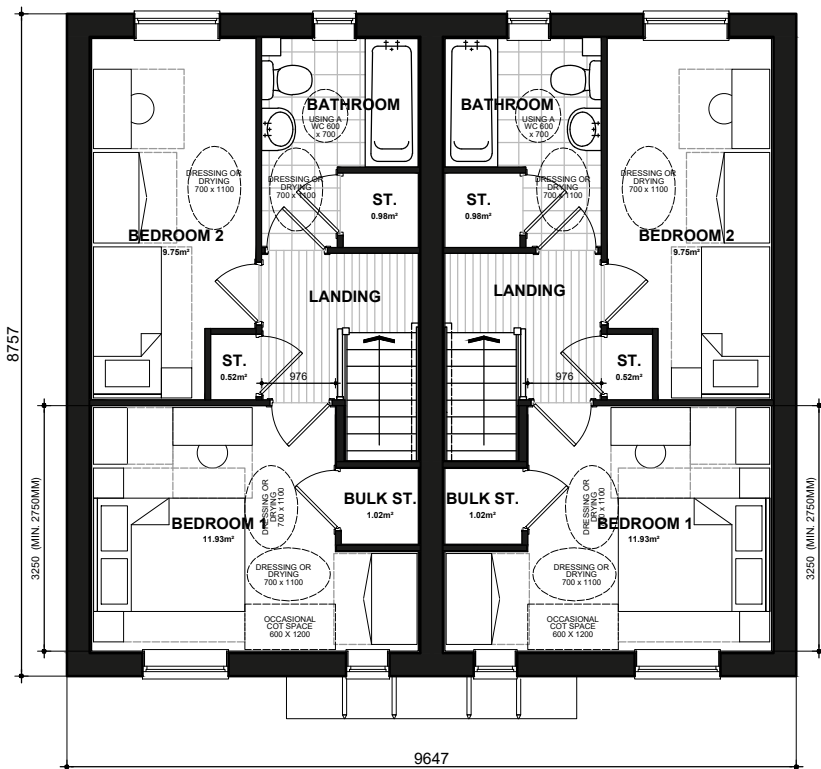
QANSS.2B3P-1
REAR ELEVATION

QANSS.2B3P



NSS.2B3P
GROUND FLOOR PLAN

NSS.2B3P-1



NSS.2B3P
FIRST FLOOR PLAN

NSS.2B3P-1

National Space Standards		
	Actual Area	NSS Requirement
Ground Floor Plan	35.01m²	
First Floor Plan	35.01m²	
Total floor area m²	70.01m²	70.00m²
Total floor area ft²	754ft²	753ft²
Bedroom 1	11.93m²	11.5m²
Bedroom 2	9.75m²	7.5m²
Bulkhead Store	1.02m²	
Bathroom Store	0.98m²	
Landing Store	0.52m²	
Built in store total	2.52m²	2.0m²

SILKIN
QA BRICK
NSS.2B3P.PL-03

DATE: JUN 2023
SCALE: 1:100 @ A3

DRAWN: MW
CHECKED: GPM

REV:

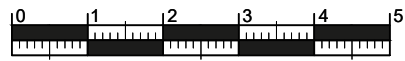
70.01m²
754ft²

**FEATURED
PLOT**

BLOOR HOMES

NSS.2B3P - HQI Unit Type: 67-75sqm.

Phase B : Plot 142 & 143



Scale 1:100
Issue 4

*HQI scores are based on minimum requirements:-
Unit Size - 41, Unit Layout - 32, Unit Services - 22



QANSS.2B3P
FRONT ELEVATION

QANSS.2B3P

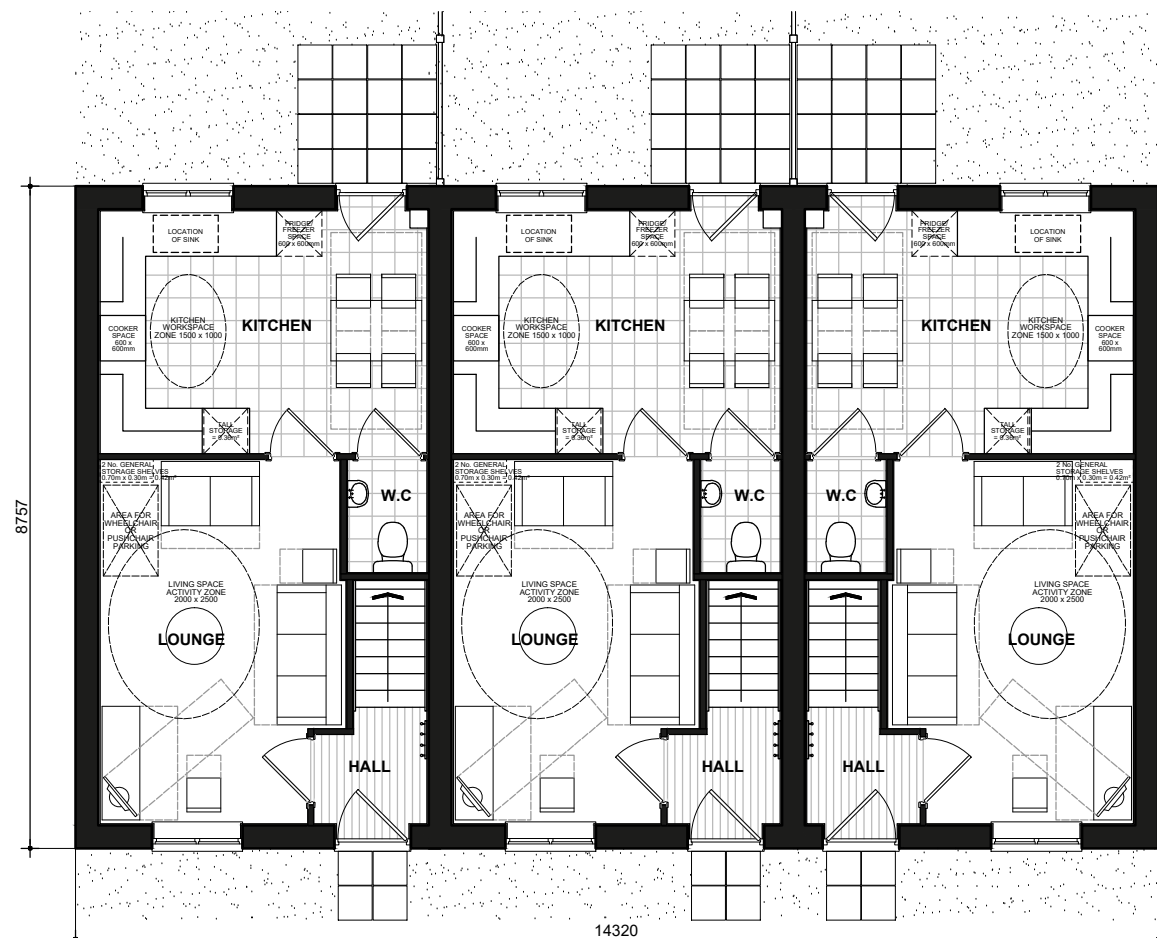
QANSS.2B3P-1

QANSS.2B3P-1
SIDE ELEVATION

QANSS.2B3P-1
REAR ELEVATION

QANSS.2B3P

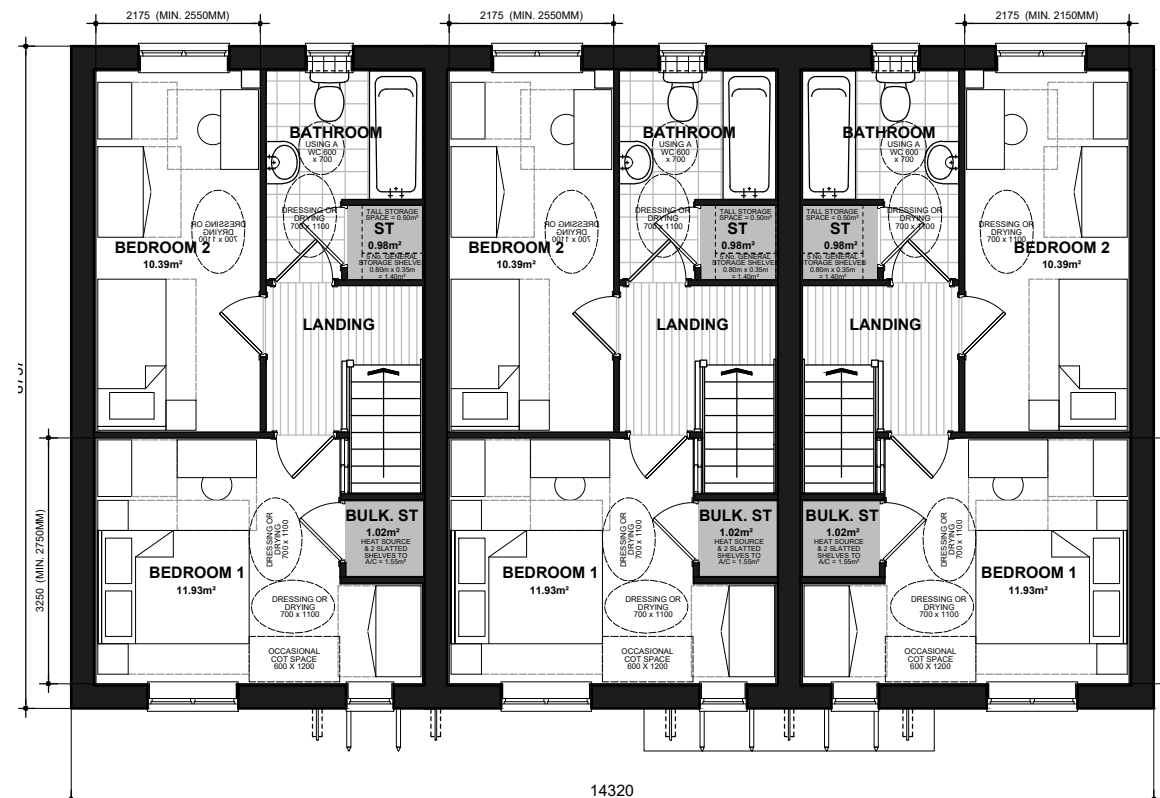
QANSS.2B3P



NSS.2B3P
GROUND FLOOR PLAN

NSS.2B3P

NSS.2B3P-1



NSS.2B3P
FIRST FLOOR PLAN

NSS.2B3P

NSS.2B3P-1

National Space Standards		
2B3P	Actual Area	NSS Requirement
Ground Floor Plan	35.01m²	
First Floor Plan	35.01m²	
Total Floor Area m²	70.01m²	70.00m²
Total Floor Area ft²	754ft²	753ft²
Bedroom 1	11.93m²	11.5m²
Bedroom 2	10.39m²	7.5m²
Bulkhead Store	1.02m²	
Bathroom Store	0.98m²	
Built in Store Total	2.00m²	2.0m²

Phase A : Plot 327-329 & 316-318

Silkin
BRICK
NSS.2B3P.PL-01

DATE: SEPT 2019 REV:
SCALE: 1:100 @ A3

DRAWN: VO 70.01m²
CHECKED: TP 754ft²

NSS.2B3P
HQI* Unit Type: 67-75sqm.
HQI 5: 60% HQI 6: 56% HQI 7: 59%

**FEATURED
PLOT**

BLOOR HOMES

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Application No: 22/2499N

Location: Land Off Minshull New Road and FLOWERS LANE, LEIGHTON

Proposal: Reserved matters application for appearance, landscaping, layout and scale with respect to 304 dwellings on Parcels C, D, E, F and G - approved outline application 19/2178N.

Applicant: Vistry Partnership Ltd

Expiry Date: 25-Oct-2022

SUMMARY

This application proposes the Reserved Matters for several Parcels of land, approved in outline and subject to a Section 106 legal agreement, under application reference 19/2178N. It is one of four separate applications being considered on this agenda.

These Parcels (C to G) make up the central part of the site, with Vistry Partnership being the applicant here. A number of changes have been made to the application since it was submitted, with the latest amendments being received at the time of writing this report.

Highways have raised no objections, subject to some points of detail being agreed by condition, and an informative. The PROW Team have no objections and the proposed footpath/cycleway provision is considered to be good.

The majority of the Ecology and Landscaping issues identified in this report have now been resolved, and although there are some outstanding matters to do with tree impacts, it is anticipated most of these can be addressed and Members will be updated on this matter.

Extensive discussions have been had regarding urban design and the Council's Urban Design Officer now feels the proposals have improved significantly and are therefore supported in design terms.

The Council's Flood Risk (LLFA) team have similarly been involved in detailed discussions with the applicant and detailed replies to matters raised have been submitted by the applicant. The LLFA's latest comments were awaited at the time of writing this report, but the main issues have now appear to have been addressed, and if necessary outstanding matters can be conditioned.

There are no objections on the grounds of affordable housing, public open space, residential standards, or environmental matters.

RECOMMENDATION

Approve with conditions

SITE DESCRIPTION

This application relates to a series of sites within the central area of this larger development site in Leighton West. All the sites are located in an area bounded by Minshull New Road to the East, Flowers Lane to the north, and to Middlewich Road and Leighton Hospital to the west. The site is broken down into the following parcels:

Parcel C & D: This site runs from the public footpath separating this site from development parcels to the south (considered under application 22/2476N) northwards along Minshull Road to Flowers Lane. The western site boundary would consist of an open area defined by the overhead power lines.

Parcel E: This is the western most site south of the hospital and wrapping around an existing farm complex which fronts Middlewich Road.

Parcel F: This parcel of land lies closest to the main entrance to Leighton Hospital, and would be sited adjacent to the commercial site which formed part of the overall outline approval. The site would have a frontage to the new road and back onto the open area referenced above.

Parcel G: This most northerly of these sites lies to the east of the hospital and has boundaries to Flowers Lane and a new road running south west.

There are hedgerows with some trees to the site boundaries, and some scattered trees with the sites themselves, however they are limited in number and the sites are generally open in character.

There are no conservation areas on or adjoining the site, and there are no listed buildings on or adjacent to the site.

As noted above Leighton footpath FP3 forms the southern boundary of Parcel C and Leighton footpath FP1 runs north of Parcel G, although neither cross the development areas of the sites.

All sites fall in Flood Zone 1 – Least risk of flooding.

PROPOSAL

The application title reads:

“Reserved matters application for appearance, landscaping, layout and scale with respect to 304 dwellings on Parcels C, D, E, F and G - approved outline application 19/2178N.”

Reserved Matters approval is sought for Appearance, Landscaping, Layout & Scale.

The site makes up the northern part of LPS4 - the southern part being applied for under reference 22/2476N also on this agenda).

The main elements of the proposed development consist of:

Vehicular access is proposed at various points along the new road being built as part of the North West Crewe Package referenced below. Pedestrian access is proposed at various points to the individual parcels.

304 Residential Units (Use Class C3) broken down as follows:

- 273 Private dwellings a mix of 2, 3, and 4 beds
- 31 Affordable dwellings a mix of 2 & 3 beds

Number of Beds	Number of Units	%
Private Dwellings		
2	46	17%
3	104	38%
4	123	45%
Affordable Dwellings		
2	21	68%
3	10	32%

Table 3: Parcels C- G Schedule of Accommodation

Finally, a series of play areas are proposed as part of these phases of the development, with a concentration in the proposed central linear open area which passes the sites, referenced above, which would form the main areas of open space etc applied for in application referenced 22/3228N also on this agenda.

An Environmental Impact Assessment (EIA) was submitted with the outline application.

RELEVANT PLANNING HISTORY

Outline planning permission was granted under:

19/2178N Outline planning approval for the development of up to 850 residential units (Use Class C3), land reserved for new primary school, a local centre (Use Class A1-A4, AA, B1a, C3 and D1) and associated infrastructure and open space. Land off Minshull New Road and FLOWERS LANE, LEIGHTON APPROVED Nov 2021

In addition, planning approval was granted for a series of Highway works in the immediate vicinity:

18/6118N A proposed series of highway infrastructure measures and associated works, in the Leighton area of Crewe, and known as the North West Crewe Package - Land Between Flowers Lane Minshull New Road The A530 Middlewich Road And North Of, PYMS LANE, CREWE APPROVED March 2019

Finally, this is one of four reserved matters applications submitted for Leighton West. All are on this agenda. The others are:

22/2476N Reserved Matters application for approval of appearance, landscaping, layout and scale following Outline approval 19/2178N for erection of 304 dwellings on Parcels A and B. Land Off Minshull New Road And, FLOWERS LANE, LEIGHTON

22/2500N Reserved matters application for appearance, landscaping, layout and scale with respect to 200 dwellings on Parcel H. An Environmental Statement was submitted at the time of the outline application.. Land Off Minshull New Road And, FLOWERS LANE, LEIGHTON

22/3228N Reserved matters application for appearance, landscaping, layout and scale with respect to green infrastructure and open space across the outline application area - approved outline application 19/2178N. Land Off Minshull New Road And, FLOWERS LANE, LEIGHTON

Other approvals under LPS 5, to the north include:

16/2373N Outline application for the construction of up to 400 dwellings with garaging; parking; public open space; landscaping; new vehicle and pedestrian accesses; highway works, foul and surface water drainage infrastructure and all ancillary works. - Land At, Flowers Lane, Leighton APPROVED January 2018.

20/3210N Reserved matters approval sought for access, appearance, landscaping, layout and scale. following outline permission for the construction of up to 400 dwellings with garaging; parking; public open space; landscaping; new vehicle and pedestrian accesses; highway works, foul and surface water drainage infrastructure and all ancillary works. Land At, FLOWERS LANE, LEIGHTON APPROVED July 2021

This development is now on site.

Finally, there are numerous approvals at Leighton Hospital, the most relevant recent approval being a car park on the north eastern side fronting Flowers Lane.

POLICIES

Cheshire East Local Plan Strategy (LPS) – 2010-2030

PG1 – Development Strategy
PG6 – Open Countryside
SD1 - Sustainable Development in Cheshire East
SD2 - Sustainable Development Principles
IN1 – Infrastructure
IN2 – Developer Contributions
SC1 – Leisure and recreation
Sc2 – Indoor and outdoor recreation
SE 1 - Design
SE 2 - Efficient Use of Land
SE3 – Biodiversity and Geodiversity
SE 4 - The Landscape
SE 5 - Trees, Hedgerows and Woodland
SE 6 - Green Infrastructure
SE 13 - Flood Risk and Water Management
CO1 – Sustainable Travel and Transportation

LPS4 – Leighton West

LPS5 – Leighton

Site Allocations and Development Policies Document (SADPD) 2022

GEN 1 - Design principles
GEN 4 - Recovery of forward-funded infrastructure costs
ENV 1 - Ecological network
ENV 2 - Ecological implementation
ENV 5 – Landscaping
ENV 6 - Trees, hedgerows and woodland implementation
ENV 7 - Climate change
ENV 14 - Light pollution
ENV 16 - Surface water management and flood risk
HOU 1 - Housing mix
HOU 8 - Space, accessibility and wheelchair housing standards
HOU 13 - Residential standards
HOU 14 - Housing density
HOU 15 - Housing delivery
INF 1 - Cycleways, bridleways and footpaths
INF 3 - Highway safety and access
INF 6 - Protection of existing and proposed infrastructure
INF 9 – Utilities
REC 3 - Open space implementation
REC 5 - Community facilities

Neighbourhood Plans:

Crewe has not made any progress towards making a Neighbourhood Plan, and Minshull Vernon is at Regulation 7 Stage: Designated Neighbourhood Area, but at this stage can be given very little weight.

Other Material Considerations

The National Planning Policy Framework

CONSULTATIONS (External to Planning)

United Utilities: Whilst originally objecting to the application, they have now confirmed the applicant's more recent submissions have addressed their concerns and they raise no objections subject to informatives concerning asset protection.

SP Energy Networks: No objections are raised, but they remind the applicant of their health & safety responsibilities around the significant electricity infrastructure which passes through the site, and the safe working practices that should be employed.

Cadent Gas – No objections subject to informatives

CEC Head of Strategic Infrastructure: No objections subject to conditions and an informative.

CEC Housing: Whilst originally raising objections to the application, as there was uncertainty about the affordable numbers and mix, following the submission of additional supporting information Housing have now confirmed they have no objections to the application.

CEC Public Rights of Way: The PROW Team have confirmed no PROW is affected by the development.

CEC Environmental Health: No objections are made, subject to reminding the applicant of their responsibilities to discharge the conditions under the outline approval relating to amenity and air quality, and the contaminated land team recommends a further condition. Informatives are recommended.

CEC Flood Risk Manager: Extensive discussions have taken place and updated comments are awaited at the time of writing the report.

ANSA: Now that the range of facilities has been clarified across the site they raise no objections, however they recommend the detailed design of facilities is conditioned.

Archaeology: No objections are made but a condition is recommended as set out in the report below.

VIEWS OF THE TOWN/PARISH COUNCILS

Minshull Vernon and District Parish Council: No comments received

Crewe Town Council: “The committee objects on the following grounds:

- I. That the site does not provide for adequate amenity and play facilities for the scale of development
- II. That the scale of the development will present an unsustainable burden on social infrastructure (e.g. schools, health)
- III. That net biodiversity gain has not been established or evidenced, which therefore does not meet Cheshire East Planning policy”

OTHER REPRESENTATIONS

Four local residents have written in raising the following issues:

- All houses should be built to high environmental standards incorporating heat pumps solar panels etc.
- Homogeneity of design – what about opportunities for self build?
- Concern about traffic congestion issues in the area
- Need for community facilities and shops in the area

OFFICER APPRAISAL

Principal of Development

The site is allocated in Local Plan policy LPS 4 and has the benefit of outline planning permission as set out above. The outline set out the requirements for the site, and the Section 106 contributions/requirements. The policy reads:

LPS 4 Leighton West, Crewe

Development at Leighton West will result in the creation of a new sustainable neighbourhood to support Leighton Hospital and provide a key site for the development of an automotive research, development and supply hub, in partnership with Bentley Motors which is located in very close proximity to the site. The development of Leighton West over the Local Plan Strategy period will be achieved through:

1. Contributions to health infrastructure and the provision of land adjacent to Leighton Hospital, for its future expansion, to ensure that the future health care needs of the area can be met;
2. The delivery of around 850 new homes (at a variety of densities). The design, density and scale of the development should reflect the fact that the site lies in a transitional location between the higher density urban area and the rural area. The surrounding development is predominantly suburban and the development of the site should reflect this. The development of the site will be masterplan-led, including a design code, which will consider its location, constraints and opportunities;
3. The inclusion of key worker housing, for the employees of Leighton Hospital;
4. The delivery of a new mixed-use local centre that will serve Leighton Hospital and nearby residents including:
 - i. Provision of retail appropriate to meet local needs;
 - ii. Community facilities;
 - iii. Public House;
 - iv. Children's day nursery;
 - v. A new primary school; and
 - vi. Leisure facilities;
5. Around 5 ha of additional employment land located at the southern end of the site including a science/energy park which could include advanced/automotive engineering and manufacturing;
6. The incorporation of green infrastructure, including:
 - i. Green corridor;
 - ii. Allotments;
 - iii. Open space including formal sports pitches, multi use games area, outdoor gym, equipped children's play space and facilities for teenagers;
7. A new bus interchange for the hospital and nearby residential areas;
8. On land to the north of Pym's Lane, there is potential for a science/energy park and delivery of a key site for the development of an automotive research, development and supply hub, working in partnership with Bentley Motors. This area also has the potential to include a geothermal plant and district heating hub;
9. The widening and/or realignment of Smithy Lane, to provide access to the site and improved access to Leighton Hospital for emergency vehicles and suitable footpath and cycle lanes;
10. A package of network improvements to provide improved access to Leighton Hospital and surrounding residential development including emergency vehicles and suitable footpath and cycle lanes; and
11. An improved 'emergency portal' for blue light vehicles being able to access the hospital's Accident and Emergency Department from Smithy Lane.

Outline approval has been granted for the principle of the development for which detailed consent under this reserved matters application are now sought. The application is considered to comply with the relevant elements of this policy.

Highway Implications

This site forms part of the local plan allocation LPS4 and already has outline approval where access was determined including the wider highways impact and any mitigation required. This current proposal is the reserved matters for the several parcels of development to the east and south of the hospital.

The various parcels of land will be accessed off the new highway infrastructure approved in a previous application. The streets directly off the approved highway infrastructure will have adequate width to cater for the development traffic with street widths decreasing the further into the site they are but remaining within adoptable requirements.

Parcel F is adjacent to what will be the local centre which will contain some commercial units but details of this will be subject to a separate application. The carriageway serving this and the adjacent apartments and residential units is sufficient, and includes footways for pedestrian, including connections to the cycle network that will run through the site. There does not appear to be any cycle parking for the apartments, and this should be conditioned.

The largest parcel of land on this application is that which runs through what is currently Smithy Ln and south parallel to Minshull New Rd. The main road through this will include raised tables at various locations to assist in managing vehicle speeds. It will also include pedestrian connections to Minshull New Rd to the east and to the new local centre to the west.

Smithy Ln will become a cycle/pedestrian route for a large part of it as shown on the plans but there are a number of statutory undertakers that will need access onto here including emergency access at all times. The entrance to Smithy Ln has been designed to therefore allow access but is discouraged from general use, and in addition there will be a TRO installed allowing for access only.

Plot 16 has no turning area and a long reversing distances and this should be conditioned. A toucan crossing, across the new section of highway, from the new local center to the hospital is required and this can also be conditioned. A small number of property driveway lengths will need increased slightly to prevent parked cars overhanging the highway. This is a minor amendment and can be conditioned.

Public Rights of Way/Cycle routes:

The PROW Team have confirmed that no PROW is affected by this application.

In terms of wider pedestrian/cycle access there are two main elements of provision. Firstly, there are the pedestrian/cycle routes which run alongside the new roads which make up the new highway network in the area and provide the primary access to all these parcels. These help link the sites north-south, to the hospital and beyond to Parcel H adjacent to the proposed school and associated open space facilities. Links to Crew Town are also improved.

Reenforcing these highway links are those now proposed through these applications including the main footpath/cycle ways following the green infrastructure corridor north south and linkages with individual parcels linking residents eastwards towards Crewe Town Centre and across the site itself.

The overall provision is considered to excellent and should encourage residents to walk and cycle. In line with policy objectives.

Landscape

The Council's Landscape Architect makes the following observations:

Parcel C No objections.

Parcel D No objections but he would prefer the bulking out of the tree planting in the green space next to the junction to provide a small green space which is screened from the large road network.

Parcel E No objections but the officer finds the front entrances and footpath layouts to units 15-20 peculiar and in his view a touch poorly laid out with the front access to properties being through a small fenced ally. Likewise unit 1. Planting close to the roads should be bulked up and maybe the introduction of some earthwork bunding to help soften any carriageway noise.

Parcel F Some of the boundary planting could do with bulking up.

Parcel D No objections but the officer would prefer the bulking out of the tree planting in the green space next to the junction to provide a small green space which is screened from the large road network.

Parcel E No objections but the officer finds the front entrances and footpath layouts to units 15-20 peculiar and in his view a touch poorly laid out with the front access to properties being through a small fenced ally. Likewise unit 1. Planting close to the roads should be bulked up and maybe the introduction of some earthwork bunding to help soften any carriageway noise.

Parcel F Some of the boundary planting is a touch underwhelming and sparse, however the officer accepts that the wayleaves of the wires has an effect.

Parcel G No objections but the officer has concerns regarding the wider footpath network design and integration with surrounding parcels both proposed and future. Something that has been tabled with the applicant team in a recent meeting that is considered underwhelming and sparse, however it is accepted that the wayleaves of the wires has an effect.

Parcel G No objections but the officer has concerns regarding the wider footpath network design and integration with surrounding parcels both proposed and future. Something that has been tabled with the applicant.

Following this meeting, the applicant has looked at these comments and has submitted a Landscape Masterplan which clarifies a number of strategic issues, in particular the footpaths which the Landscape Architect was particularly concerned about. He now feels the general layout is acceptable and approach is acceptable, but that detailed landscaping should be conditioned.

Trees

An amended Arboricultural Impact Assessment (AIA) and an updated Arboricultural Method Statement (AMS) has been submitted further to the earlier forestry comments. Clarification has been provided regards several of the issues raised as requiring more detail and explanation.

It is understood that tree and hedgerow losses which have occurred since the preliminary survey was carried out at Outline have arisen because of Highway Infrastructure projects and the supporting plans are now annotated to confirm where this is believed to be the case.

The updated AIA and AMS has now been broken down by phase of development and makes provision to show all trees including those presently shown for removal shown by tree quality category colour with RPAs, so that impacts can be better understood. Some explanatory notes have been provided regards the pre agreed loss of TPO'd trees and those trees already removed for the highway's infrastructure projects.

The AIA indicates that of the trees surveyed across the wider site that 1 individual high quality A Category tree, 3 individual moderate quality B Category trees, 4 individual low-quality C Category trees and 1 poor quality U Category tree as well as a combined total of 112 linear metres from 2 hedgerows are proposed for removal to accommodate the proposals associated within Parcels C, D, E, F & G of the development. An updated AMS (Version 3) has also been provided.

The submitted Assessment of Important Hedgerows considers the whole of the site and has not been broken down to impacts on the submitted applications. Twelve hedgerows (H1-H12) have been identified as important in accordance with criteria 5a of the Hedgerow Regulations.

5. The hedgerow:

a) is recorded in a document held at a Record Office as an integral part of a field system pre-dating the Inclosure Acts

Hedgerow 1 is also importance in accordance criteria 1.

1. Any hedgerow that marks the boundary or part of the boundary, of at least one historic parish or township; and for this purpose, "historic" means existing before 1850.

The assessment confirms that sections of hedgerow H2, H5, H6, H7 AND H8B0 are to be removed as part of proposed development for Northwest Crewe Package and includes the removal of sections for Highway works. Additional removal of H3 is required for a bus route and access. The overall development will arise in the partial loss of hedgerows which form a 19th Century field system but most of the hedgerows are shown for retention where possible and the report has concluded that the overall harm is; 'less than substantial'. Mitigation is proposed of 2,300m of new hedgerows. As hedgerows have been found to be 'Important' under the above referenced criteria of the Regulations, the loss of the hedgerows to accommodate the development is a material consideration in the determination of the application although it appears that sufficient compensatory planting is being offered across the whole development area.

Having appraised the amended detail and proposals identified it's no clearer as to what the rationale is behind the loss of moderate quality B Category trees; T129 in Parcel C. High quality A cat tree (T95) is shown for removal but given the proximity of new adopted highway it is accepted that's its retention is not feasible.

Initial observations are as follows; the relationship between high quality A Category tree (T90) and moderate quality B Cat tree (T91) in Parcel G and plots 16 and 20-22 could arise in shading and dominance issues to the dwellings which are sited approximately 12 metres from the trees.

Further detail should be provided as to why tree T129 could not be retained as it would be useful to understand why minor reconfiguration of footpaths and engineer designed solutions are not possible to

facilitate the retention of this tree. The updated AMS does show all trees according to their surveyed quality category and some projected RPAs although these are not present for trees shown for removal.

The AMS as submitted does include a tree protection plan, make provision for a methodology for working in relation to retained trees, and identifies areas where special measures would be implemented in respect of minor root pruning and construction of engineer designed surfaces. Notwithstanding this, considering the tree losses which have already been carried out because of the Highway improvements and further losses proposed arising from this layout, opportunities exist to retain further trees. Separation between trees T90 and 91 and the closest plot should be increased and minor amendments to the landscape layout considered in respect of pedestrian pathways and trees shown for removal to see if additional moderate quality trees such as T129 could be retained and to provide more confidence that the most significant trees on the site will have a sustainable relationship with residential property in the longer term.

The applicant has been looking at the impacts on trees identified above as part of the revised plans, as whilst the Council's Tree Officer has not formally objected to the application on the basis of tree impacts, at least some of these impacts would seem to be avoidable. At the time of writing this report the applicant was discussing amendments with the Council's Tree Officer which would allow for the retention/better protection of more trees, and Members will be updated on this matter in an Update Report.

Ecology

A number of conditions were attached to the outline consent at this site.

Condition 9 Ecological Monitoring Strategy:

Monitoring proposals are required by this condition are included in the submitted Habitat Management Plan TEP Version 3.0 dated 10.4.24.

Condition 11 Detailed design of attenuation ponds:

No detailed designs of attenuation ponds have been submitted with this reserved matters application. The applicant has confirmed that no attenuation ponds are proposed under this reserved matters application.

Condition 12 Updated hedgerow impact assessment, mitigation and compensation strategy:

The submitted biodiversity assessment (January 2024) however advises that 2.6km of hedgerow would be lost across the entire outline site under the various reserved matters applications, with 2.17km of new hedgerow planting being provided. The originally submitted BNG metric concluded that the reserved matters applications would result in a net loss of 10.41% in respect of hedgerow biodiversity.

It was advised that the development proposals must ensure that the extent of hedgerow retained as part of the development is maximised and that any unavoidable losses of hedgerow are adequately compensated for by appropriate native species hedgerow planting. A greater length of replacement planting to that lost will be required to fully address the loss of biodiversity from hedgerow removal. Ornamental hedgerow planning is not acceptable as compensation for the loss of native species hedgerow.

Following the submission of further information to clarify the impacts of this development as opposed to losses for the road scheme – which appears to have caused the confusion, the Council's Ecologist comments:

“The proposed reserved matters application combined will result in the loss of 0.38km of hedgerow, including lengths of hedgerow considered to be ‘Important’ under the Hedgerow Regulation. The BNG metric has been undertaken on the basis of 2.17km of new hedgerow planting being provided on site. This is based on PGLA Landscape Masterplan Rev 11.

The proposed hedgerow planting would result in a net gain of hedgerows of 30.57%. Therefore, if the loss of the existing hedgerows is considered unavoidable sufficient planting would be provided to compensate for that lost. This would however be dependent upon the reserved matters application being brought forward in accordance with the landscape master plan.”

This masterplan would, should planning permission be granted, form one of the approved plans.

Condition 25 Updated badger and barn owl survey and mitigation strategy:

Acceptable updated barn owl and badger survey and mitigation strategy has been submitted as required by this condition.

Condition 26 Details of any proposed lighting. The lighting strategy should reflect the Bat Conservation Trust Guidance Note:

The covering letter from the Agent refers to a lighting strategy being included in the submitted ‘Ecological Monitoring Strategy’ however an ‘Ecological Monitoring Strategy’ does not appear to have been submitted in support of this application. The applicant proposes that a detailed design for the lighting (in accordance with the strategy) be submitted under condition 22 prior to installation of the lighting.

Condition 27 25 year habitat management plan:

An acceptable Management plan has been submitted (Habitat Management Plan TEP Version 3.0 dated 10.4.24).

Condition 28 The first reserved matters application shall be supported by a strategy for the incorporation of features to enhance the biodiversity value of the proposed development:

A Biodiversity Impact Assessment (TEP June 2022) which covers the full extent of the outline planning consent subject to the various current reserved matters applications was previously submitted in support of this reserved matters application. A revised BNG metric dated 22/1/24 has also now been submitted. The metric includes measurements for the areas of various habitats to be provided on site. The area of species rich grassland provided on site is less than that required by condition 28 as is the extent of hedgerow planting. The area of cornfield annuals and wet grassland is however greater than required by condition 28.

The assessment concludes that the proposed development (based on all of the reserved matters applications) would deliver a net gain for area-based habitats of 12%, but originally a net loss of -10.41% of hedgerow biodiversity. This has subsequently been amended as discussed above.

Proposals for the provision of bird and bat boxes and gaps in garden fences have been submitted as required by this condition. It must however be ensured that bat/bird boxes are proposed to be attached to 30% of consented dwellings in accordance with the Cheshire East Design Guide.

Detailed method statements for the creation of species rich grassland and cornfield annual meadow habitats are outstanding. These must be submitted prior to the discharge of this condition.

Bat survey

Two additional trees have been identified as requiring removal under this series of reserved matters application (T82 and T122). Application 22/3228n is supported by a letter from the applicant's ecological consultant advising that a bat survey/assessment of these trees is required. One of these trees however appears to fall within the boundary of 22/2499N. A further inspection of these trees has been undertaken and the trees have been found to be unsuitable for roosting bats. No further action in respect of roosting bats is therefore required.

Additional conditions

If reserved matters consent is granted a condition would be required to safeguard nesting Birds.

Whilst ideally all matters would have been resolved now, outstanding matters relating to the discharge of conditions highlighted above, will be picked up in their subsequent discharge.

Urban design

The development has been assessed as a whole (considering the four Reserved Matters (RM) applications collectively) using the Building for a Healthy Life (BHL) considerations. Performance is summarised in the table below. A more detailed commentary in relation to each of the considerations has also been provided, but not included within this report.

Integrating into the Neighbourhood				Distinctive Places				Streets for All			
1 Natural Connections	2 Walking, cycling public transport	3 Facilities and services	4 Homes for everyone	5 Making most of what's there	6 Memorable character	7 Well defined streets/spaces	8 Easy to find your way around	9 Healthy streets	10 Cycle and car parking	11 Green and blue infrastructure	12 Back of pavement, front of home
G	G	G	G	G	A	G	G	A	A	A	A

Summary/conclusions

This is a large and complex scheme compounded by the submission of several separate ARM applications, prepared jointly by the two developers. Significant input has been necessary to improve the quality of the originally submitted proposals. Key aspects are:

- Securing a more responsive, forward-thinking approach to the design of the local centre and other parcels within proximity to Leighton Hospital.
- Ensuring the residential component of the local centre acts as a positive lead for the design and development of the future commercial area including securing detailed coding to help guide the design of the commercial area and associated public realm.
- Developing a character area approach for the remainder of the scheme to ensure a degree of continuity and consistency between developers but also some localised individuality (albeit this could have been taken a little further).
- Ensuring there is a clear street hierarchy, with tree lined avenues within the larger parcels and stronger, more formal gateways into smaller ones, with a supplementary hierarchy of streets and spaces, appropriately detailed and surfaced to suit their function.
- Ensuring appropriate levels of landscaping along the edges of development parcels.
- Inclusion of feature spaces within layouts to help reinforce the social function of streets, better handle concentrations of parking and to create points of incident and interest.
- Seeking to integrate existing landscape features within the layout whilst accepting the constraints imposed in certain locations and the need to connect to adjoining developments and the approved highway improvement scheme (Northwest Crewe package currently under construction).

- Ensuring the inclusion of blue infrastructure as a positive element of the place creation adhering to principles within the recently adopted SuDS Design Guide SPD, with a series of basins, swales and ditches for the outlying development areas and within the main central area of POS, and a more urban approach for the local centre, including living surfaces to buildings and structures, rain gardens and channels/rills within streets and spaces.
- Improved connectivity within the development and to the surrounding area with integrated public rights of way, combined footpath/cycleway routes and stronger interrelationship and connectivity to, within and along the central main POS area defined by the pylon route.
- Enhancements to the strategic approach to landscape design, including reflecting and building upon the character of the spaces, integrating existing landscape features, provision of varied play and leisure opportunity and inclusion of local food growing, including a central allotment area, orchard and on the doorstep, more informal, food growing within and adjacent to housing.

Whilst in certain respects the design response could have gone further/been more creative, the work invested has enhanced the scheme considerably from the initial submissions and will lead to a better quality and more cohesive development that responds more favourably to policy LPS4 (Parcels A-G) and LPS5 (Parcel H) of the CELPS, local design policy and guidance and the ambitions of the spatial design code submitted with 19/2178N. The proposals are therefore supported in design terms.

Residential standards

As the proposed development of these parcels are well separated from adjacent housing areas on Minshull New Road, and to a farm complex to the west there are no issues with separation distances in this regard. Parcel F adjoins the hospital complex, but at this point there is an area of parking – separated by an established hedgerow so it is not considered there are any significant issues here.

Within the development parcels themselves the majority of the properties meet or exceed the required separation distances, and in the few cases where they don't the properties are slightly off set from one another or there are good urban design reasons for it.

Environmental Impacts

Environmental Protection have confirmed they have no objections to the application with regards to general amenity (including noise) and air quality subject to satisfactory discharge of the relevant conditions on the outline and informatives. The contaminated land team comment that they have no objection to the application subject to the following comments with regard to contaminated land:

- RoC Consulting Phase 1 Desktop Study was submitted in support of the outline application.
- The report identifies a number of potential pollutant linkages that require further investigation. The report recommends that a Phase II investigation is conducted at the site.
- A rigorous monitoring-based ground gas risk assessment will be required to assess the potential impact of ground-gas migration from the landfills adjacent south.

As such, and in accordance with the NPPF, the Contaminated Team recommends that that conditions, reasons and notes be attached should planning permission be granted.

Flood Risk/Drainage

Extensive discussions have taken place with the applicant's drainage engineers, with matters of relating to drainage calculations, finished floor levels, management arrangements and details of a pumping station being discussed. The applicant has submitted detailed replies to all matters raised, but at the time of writing this report the Flood Risk Manager's updated comments are awaited. It is considered that if there are any outstanding matters raised, they could be conditioned.

Public Open space

The public open space provision was set out at the outline stage and captured by the Section 106 Agreement. Overall, the site includes the following facilities:

- Multi Use Games Area (MUGA)
- Teen Shelter & Play
- Neighbourhood Equipped Area of Play (NEAP)
- An orchard
- Allotments
- Local Equipped Areas of Play (LEAP)
- Local Areas of Play (LAP) - 6 No.
- Areas of outdoor gym equipment
- Areas of incidental open space

Whilst many of these facilities are included with the common areas of green infrastructure which runs through the centre of the site, and along the northern boundary some are located within individual parcels where it was deemed preferable to give easier access for residents.

For parcels C, D, E, F & G no play area/facilities are proposed in these (relatively small) housing areas, however immediately adjacent in the area of green infrastructure that adjoin all the sites are the proposed allotments, orchard, areas of outdoor gym equipment, areas of incidental open space and 3 LPA's.

Due to the way the information was presented, ANSA where initially unclear what provision was being made, however following the submission of a Landscape Masterplan which sets it out more clearly they have confirmed they have no objections to the provision as submitted. There are however detailed design issues with the individual areas of provision, so ANSA have asked for the detailed design of each of the items listed above (incidental open space excluded) to be subject of a condition.

Affordable Housing

The Section 106 Agreement attached to the outline approval for the site required 10% of the houses on site to be affordable – based on a viability appraisal which was independently assessed at the time. The policy compliant 35% Shared Ownership/65% Affordable Rent was also required.

Now that the applicant has clarified the provision, by the submission of updated Affordable Housing Statements for the different parcels which shows the number of units in each tenure, the number of beds and room sizes, and how they are laid out on site Housing have confirmed they are happy with the proposals. In these parcels 31 units are shown 10% of the 304 units proposed. All the properties meet the NDSS.

It is now considered that the affordable housing provision is acceptable.

Archaeology

Commenting on the applications as a whole, the Archaeological Unit write that this application is for a significant development on the land off Minshull New Road and Flowers Lane, on the outskirts of Crewe. This substantial residential and landscaping development will have a dramatic impact on the landscape and any below ground remains which may be present.

After reviewing the extensive supporting documentation for this application, which included a Heritage Statement authored by Orion Heritage, and reviewing the historical information held on Cheshire Historic Environment Record. It has become clear that there are archaeological considerations which will need to be addressed for this development.

Previous consultations provided by APAS have offered some insight into the potential archaeological remains that this proposed development will impact. A previous officer offered advice relating to application 18/6118N in 2018 noting that there are two township boundaries within the proposed development area. Furthermore the previous officer noted that the presence of the WWII crash site within the proposed development area. Mark recommended archaeological mitigation to address the township boundaries.

Another officer offered comments in 2017 in relation to a pre application consultation, and identified the presence of tithe buildings within the East of the application area, relating to Finger Post Farm. The officer at the time recommended a programme of archaeological mitigation to address these tithe buildings.

The historical statement report supplied by Orion Archaeology indicates in section 10.79 that there are no substantial archaeological remains and therefore there “No archaeological mitigation is warranted” The report does note the presence of the WWII crash site, but neglects to mention the two township boundaries, the tithe buildings in association with Finger Post Farm, or the tithe buildings located to the West of the development area. Subsequently, there clearly is a requirement for archaeological mitigation in order to address and record these tithe buildings and township boundaries.

The area of the WWII crash site, “The Wellington X3547” was recorded as crashed within the study area on 20th of March 1943. Any work in the vicinity of the crash site will require a Ministry of Defence license. There has been a previous license issued for this site, relating to the construction of the spine road, however, this has since expired and the applicant will need to apply for a new license in relation to this current proposed development. It is advised that this could form part of the program of archaeological mitigation and the license obtained on behalf of the applicant by the contract archaeological unit.

Given the presence of the tithe buildings, township boundaries and the crash site, it is clear that despite the comments in 10.79 of the Heritage Statement, there is a requirement for archaeological mitigation for this proposed development. This mitigation should include the following:

- Formal section across the northern Township boundary,
- Strip and Map of the tithe buildings associated with Finger Post Farm
- Strip and Map of the tithe buildings located in the West of the development area
- A License obtained from the Ministry of Defence

These works can be undertaken by an archaeological contractor and may be secured by condition.

Other matters

Crewe Town Council and residents have expressed concern about the lack of facilities in the area. Although this was addressed at the outline stage of the application(s) the local plan allocation (as set out above in the policy section), sets out what is required, namely a “new mixed-use local centre”. These reserved matters applications only seek permission for residential development – with associated POS/Landscaping etc, but clearly show the school site adjacent to parcel H on the most northerly site, and the local centre site adjacent to the southern side of the hospital adjacent to parcel F.

In addition, residents have expressed the need for strong environmental measures to be installed in new residences. Whilst this is understood, the environmental credentials of new build houses are controlled by current building regulations not planning.

Finally, one resident asked why self-build was not included in the proposals. Unfortunately, while the Council in principle supports an element of self-build homes, there is no requirement for all developments to contribute to this provision.

CONCLUSIONS

This application proposes the Reserved Matters for several Parcels of land, approved in outline and subject to a Section 106 legal agreement, under application reference 19/2178N. It is one of four separate applications being considered on this agenda.

These Parcels (C to G) make up the central part of the site, with Vistry Partnership being the applicant here. A number of changes have been made to the application since it was submitted, with the latest amendments being received at the time of writing this report.

Highways have raised no objections, subject to some points of detail being agreed by condition, and an informative. The PROW Team have no objections and the proposed footpath/cycleway provision is considered to be good.

The majority of the Ecology and Landscaping issues identified in this report have now been resolved, and although there are some outstanding matters to do with tree impacts, it is anticipated most of these can be addressed and Members will be updated on this matter.

Extensive discussions have been had regarding urban design and the Council’s Urban Design Officer now feels the proposals have improved significantly and are therefore supported in design terms.

The Council’s Flood Risk (LLFA) team have similarly been involved in detailed discussions with the applicant and detailed replies to matters raised have been submitted by the applicant. The LLFA’s latest comments were awaited at the time of writing this report, but the main issues have now appear to have been addressed, and if necessary outstanding matters can be conditioned.

There are no objections on the grounds of affordable housing, public open space, residential standards, or environmental matters.

RECOMMENDATION

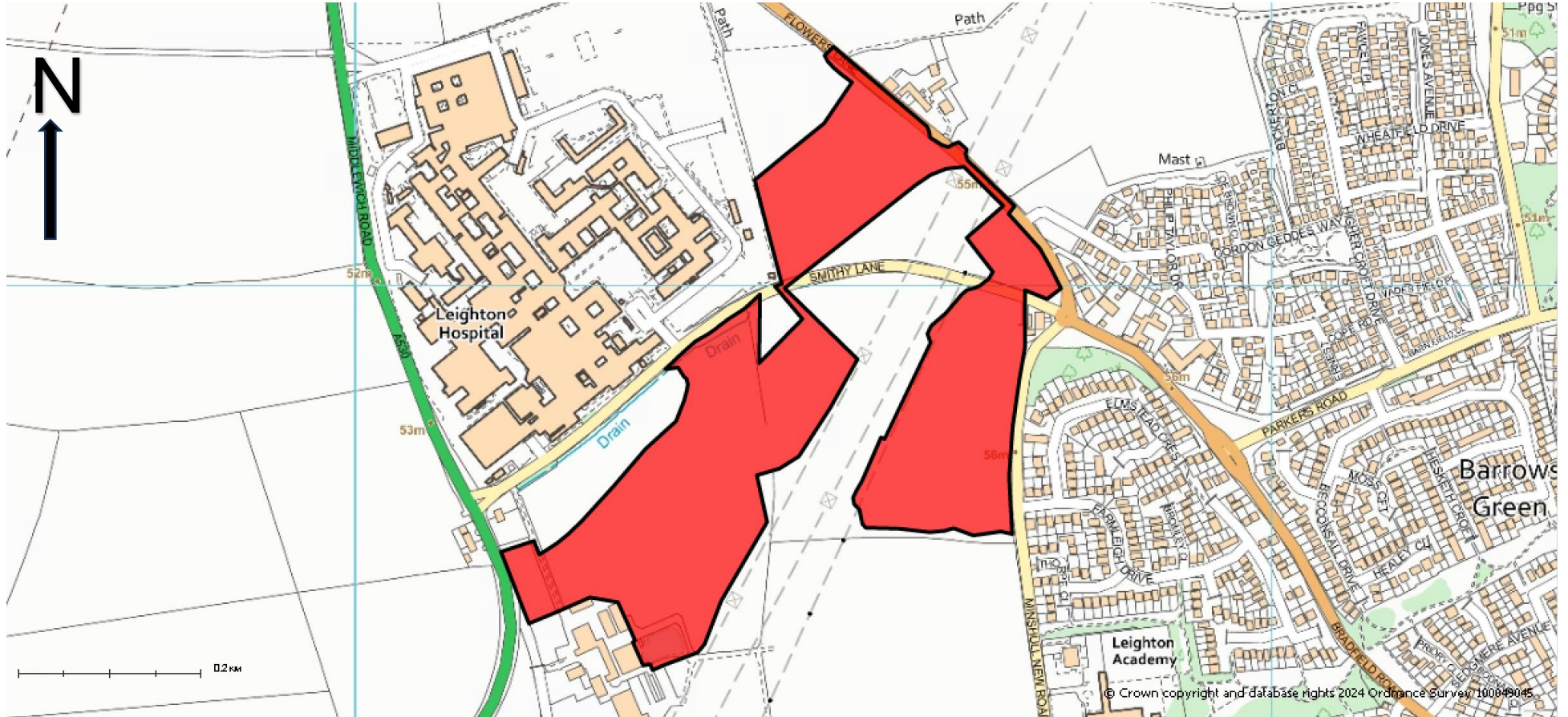
Approve subject to the following conditions;

1. Approved plans
2. Bird nesting
3. Submission of a Phase II ground investigation and risk assessment
4. Submission of a Verification Report
5. Soils testing
6. Unexpected contamination
7. Archaeological mitigation
8. Detailed design of the individual elements of the play areas/ /outdoor gyms, allotments and orchard to be agreed.
9. Detailed landscaping to be submitted and approved
10. A plan detailing covered cycle parking for the apartments should be submitted and approved.
11. A plan detailing turning area for plot 16 should be submitted and approved.
12. A plan detailing the Toucan crossing from the local centre to the hospital should be submitted and approved and provided prior to occupation.
13. A plan of the driveways of plots 47-50, 73-74 should be submitted and approved.

Informatives;

- SP Energy informatives
- Public Rights of Way
- Environment Protection Informatives
- Highways: The Smithy Ln TRO will be implemented by the applicant during the s38 technical approval stage.

In the event of any changes being needed to the wording of the Committee's decision (such as to delete, vary or add conditions/informatives/planning obligations or reasons for approval/refusal) prior to the decision being issued, the Head of Planning has delegated authority to do so in consultation with the Chairman of the Strategic Planning Board, provided that the changes do not exceed the substantive nature of the Committee's decision.



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22/2499N

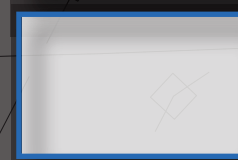
Land Off Minshull New Road
and Flowers Lane

LEIGHTON

Key



Application Boundary



Land in the Control of the Applicant

e*SCAPE10
urbanists 2009 - 2019

Project Title
Leighton West, Crewe

e*SCAPE Job No.
021-025

Client
Vistry Group

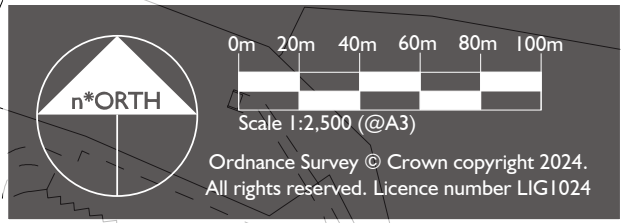
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021-025-P025

Revision
REV A

Drawing Title
Application Location Plan

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1:2,500 @ A1

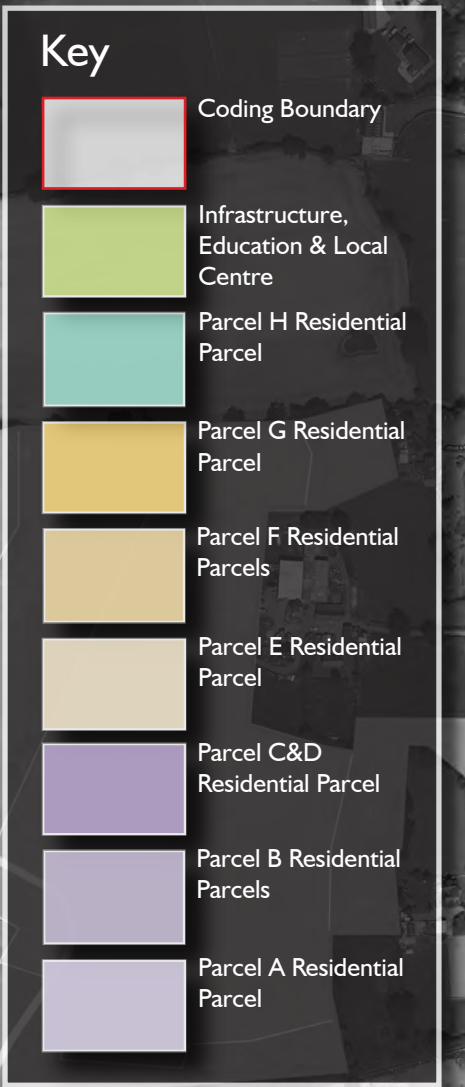
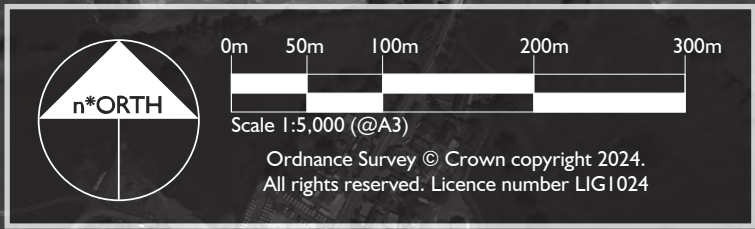
Date
February '24



0m 20m 40m 60m 80m 100m
Scale 1:2,500 (@A1)
Ordnance Survey © Crown copyright 2024.
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Leighton West Illustrative Layout
Scale 1:1000@2A0



e*SCAPE 15 years
urbanists 2009 - 2024

Project Title
Leighton West, Crewe

e*SCAPE Job No.
021-025

Client
Bloor/Countryside

Drawing Number
021-025-P007

Revision
REV A

Drawing Title
Parcel & Phase Boundaries

Scale
1:5,000 @ A3

Date
May'24

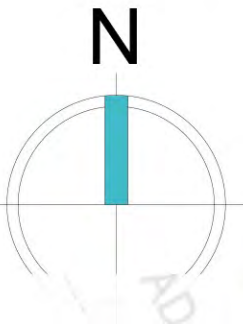


PLANNING LAYOUT KEY

- 1.8m Brick wall
- 2.5m Brick wall entrance feature
- 1.8m Hit & miss timber fence for carport (Parcel F only)
- 1.8m Close boarded timber fence
- Front boundary hedging (see landscapers spec)
- 0.45m Knee rail
- Timber gate
- Bin Storage area
- Bin collection point (bin collection day only)
- Affordable
- 600 x 600 Paving slabs (refer to surface materials plan for clarification)
- Block paving, Stretcher form (refer to surface materials plan for clarification)
- Block pavers (refer to surface materials plan for clarification)
- 1.8m x 0.9m shed position
- Tree/vegetation to be retained.
- Root protection Zone.
- Entrance pillars

Scaled @ 1:500

0 10m 20m 50m



Notes:

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All Dimensions are to be verified on site before any work commences. If any discrepancies, errors or omissions are noted, these are to be reported to STEN Architecture immediately.

If any other drawings are referenced within this layout, please refer to the specific detailed drawing for design, materials and specific working practices.

V	Surfacing to squares amended. Plot substitutions to plots 55-58. Railings removed and replaced with hedges. Footway connections shown adjacent plots 77 & 54/29. Margin position on shared surface serving plots 72-76 amended.	TS	13.06.24
U	Cycle store removed to plot 44-46	LS	26.04.24
T	Cycle storage indicated to relevant plots. Worsley FF unit granted external amenity space where applicable, adjacent garden spaces amended to suit.	LS	23.04.24
S	Affordable contribution to plots 7, 8 & 12 amended.	TS	06.03.24
R	Position of plot 9 amended and parking arrangements updated to plots 10 & 12.	TS	12.01.24
Q	Central square introduced, mix of house types to plots 9-14 amended to suit.	TS	12.01.24
P	Plots 84-116 amended	DS	24.11.23
N	Elevational treatment specification of plots 56-59, 65 & 110 amended.	LS	03.11.23
M	Additional landscaped areas shown to parking courts. Parking to plots 52-54 amended. Parking amends to plots 28-31 & 11-14	TS	27.09.23
L	General amends in line with latest round of comments from LPA. House type mix and road hierarchy updated.	TS	24.08.23
K	Bungalows removed. General updates following LPA feedback. Plot numbering updated to suit additional units.	TS	16.08.23
J	Connection to farmstead shown to southern boundary. Plots 96-99 re-orientated to suit.	TS	17.07.23
H	Plots 3-6, 38-45, 64 & 82 replanned	DS	12.07.23
G	Plots 45 & 93's boundary walls amended to be flush. Red line amended adjacent to plot 79. Plots 76-84 amended.	DS	25.05.23
F	Worsley updated to M4(3) variant	TS	17.05.23
E	Phase of development redesigned inline with comments from local authority.	TS	11.05.23
D	Schedule updated.	DS	24.03.22
C	Red line boundary removed.	DS	24.03.22
B	Red line amended. Schedules added.	DS	16.03.22
A	Phase redesigned. Kirk, Asher & Spiers house types introduced to provide M4(2) requirements	TS	28.02.22
REV:	DESCRIPTION:	BY:	DATE:

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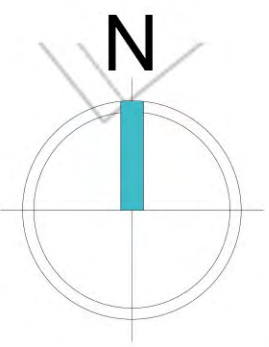
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PROJECT NO:	2150	DRAWN:	TS
DRAWING NO:	2150.PE.01	CHECKED:	SL
REVISION:			V

PLANNING LAYOUT KEY

- 1.8m Brick wall
- 2.5m Brick wall entrance feature
- 1.8m Hit & miss timber fence for carport (Parcel F only)
- 1.8m Close boarded timber fence
- Front boundary hedging (see landscapers spec)
- 0.45m Knee rail
- Timber gate
- Bin Storage area
- Bin collection point (bin collection day only)
- Affordable
- 600 x 600 Paving slabs (refer to surface materials plan for clarification)
- Block paving, Stretcher form (refer to surface materials plan for clarification)
- Block pavers (refer to surface materials plan for clarification)
- 1.8m x 0.9m shed position
- Tree/vegetation to be retained.
- Root protection Zone.
- Entrance pillars

Scaled @ 1:500

0 10m 20m 50m



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P	Block paving / paving amended.	DS	17.06.24
N	Cycle storage indicated to relevant plots. Worsley FF unit granted external amenity space where applicable, adjacent garden spaces amended to suit. Chambers to site entrance amended to 4m radii. Boundary to plots 16 & 33 amended. Boundary key updated to show hit and miss fences to carports. spec of block paving amended.	LS	23.04.24
M	Plot 39 car port updated. Plots 13-16 moved forward to create constant building line. Front gardens added to plots 21-24. Plots 29-30 moved back to increase frontage planting. Footpath connection to Smithy Lane increased to 3m. Entrance pillars moved to adjoin building.	DS	27.03.24
L	Phase of development redesigned inline with comments from local authority.	DS	13.03.24
K	Plots 15 & 16 connected to plots 13 & 14. enclosure shown between apartments and plots 25/26.	TS	11.12.23
J	Phase of development redesigned inline with comments from local authority.	TS	08.12.23
H	Phase of development redesigned inline with comments from local authority.	TS	24.08.23
G	Pump station removed.	DS	25.05.23
F	Phase of development redesigned inline with comments from local authority.	TS	11.05.23
E	Site redesigned to SK02 to incorporate pylon offset	DS	10.05.22
D	Schedule updated.	DS	24.03.22
C	Red line boundary removed.	DS	24.03.22
B	Red line amended. Schedules added.	DS	16.03.22
A	Site redesigned. Kirk, Asher & Spiers house types introduced to provide M4(2) requirements	TS	28.02.22
REV:	DESCRIPTION:	BY:	DATE:

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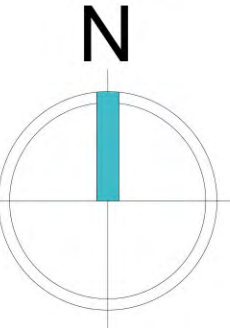
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PROJECT NO:	2150	DRAWN:	TS
		CHECKED:	SL
		REVISION:	P

PLANNING LAYOUT KEY

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- 1.8m Close boarded timber fence
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- 0.45m Knee rail
- Timber gate
- Bin Storage area
- Bin collection point (bin collection day only)
- Affordable
- 600 x 600 Paving slabs (refer to surface materials plan for clarification)
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- Block pavers (refer to surface materials plan for clarification)
- 1.8m x 0.9m shed position
- Tree/vegetation to be retained.
- Root protection Zone.
- Entrance pillars

Scaled @ 1:500

0 10m 20m 50m



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Q	Surfacing to squares amended. Railings removed and replace with hedges. Footway connection shown adjacent plot 19 & 51.	TS	13.06.24
P	Cycle store removed to plot 25.60	LS	26.04.24
N	Cycle storage indicated to relevant plots. Worsley FF unit granted external amenity space where applicable, adjacent garden spaces amended to suit, parking to plots 2-4 amended to allow for landscaping. Plot 16 to be designated as focal plot. Area of grass verge removed adjacent plot 5.	LS	23.04.24
M	Footpath connect to north amended.	DS	04.04.24
L	Additional unit added to plot 11, plots 2-5 pulled forward, parking to plots 53-54 amended. 3m wide footway removed adjacent plot 54-60.	TS	27.09.23
K	General amends in line with latest round of comments from LPA. House type mix and road hierarchy updated.	TS	24.08.23
J	Worsley replaces Becket on plot 31	DS	12.07.23
H	Red line amended adjacent to plot 25. Plots 18-25 amended.	DS	25.05.23
G	Worsley updated to M4(3) variant	TS	17.05.23
F	Phase of development redesigned inline with comments from local authority.	TS	11.05.23
E	Schedule updated.	DS	24.03.22
D	Red line boundary removed.	DS	24.03.22
C	Red line amended. Schedules added.	DS	16.03.22
B	Position of plot 17 amended. Adjacent square extended	TS	02.03.22
A	Site redesigned. Kirk, Asher & Spiers house types introduced to provide M4(2) requirements	TS	28.02.22
REV:	DESCRIPTION:	BY:	DATE:

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CLIENT:	Countryside Partnerships		
SITE:	Leighton West Crewe		
TITLE:	Proposed site layout Parcel G		
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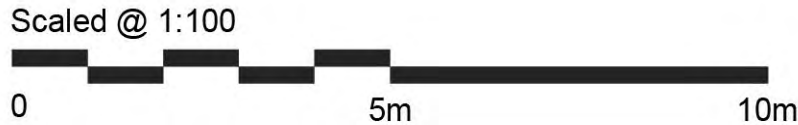
FRONT ELEVATION



SIDE ELEVATION



REAR / SIDE ELEVATION



B	Additional detailing added to blank gable of Turner type.	DS	25.04.24
A	Elevation enhanced to focal plot version.	DS	22.03.24
REV:	DESCRIPTION:	BY:	DATE:

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ARCHITECTURE

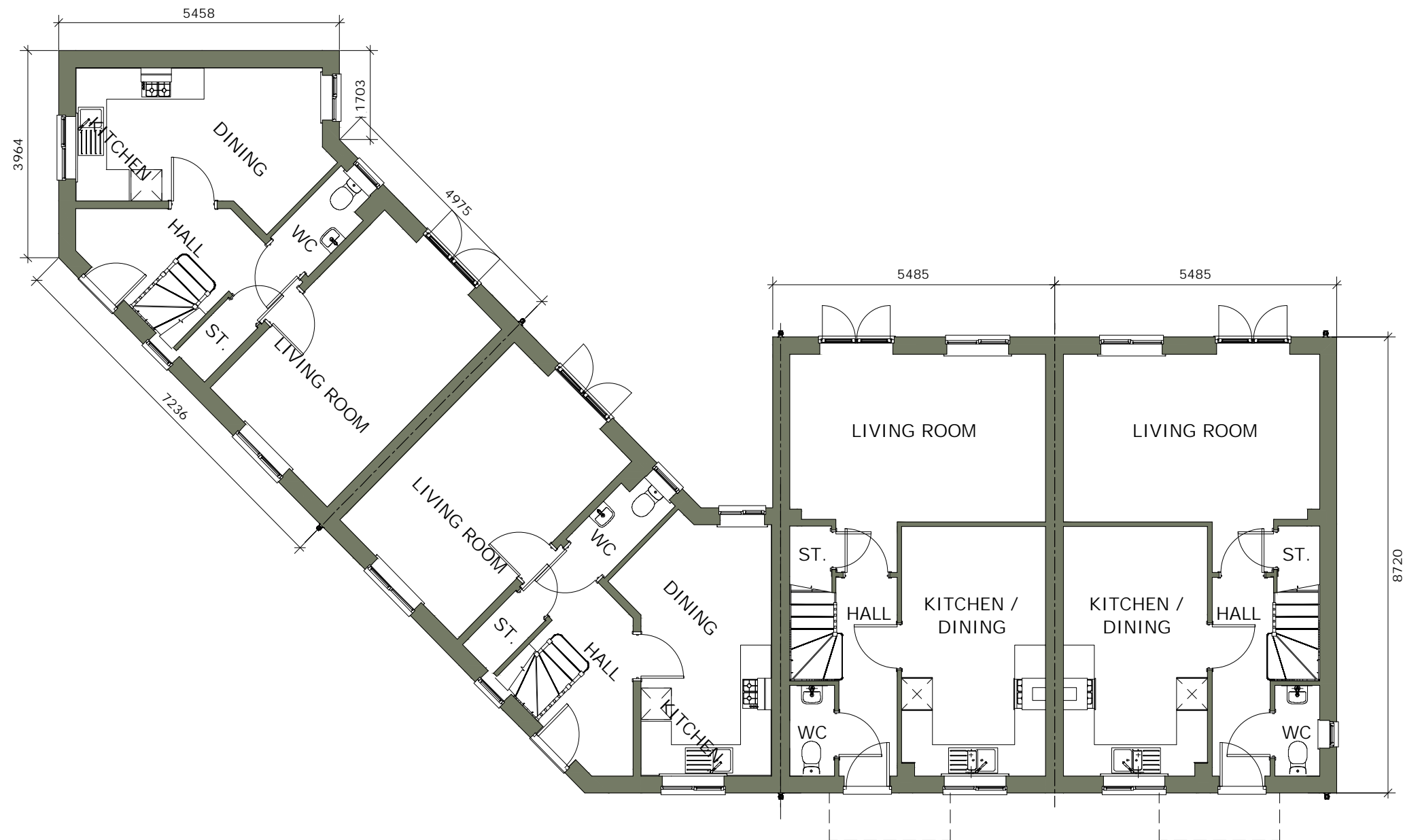
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LinkedIn: [Sten Architecture](https://www.linkedin.com/company/sten-architecture)

CLIENT: **Countryside Partnerships**

SITE: **Leighton West Crewe**

TITLE: Parcel F - Planning Drawings Aldridge / Turner 4 Block - Enhanced Elevations			
SCALE AT A3: 1:100	DATE: 05.12.23	DRAWN: DS	CHECKED: SL
PROJECT NO: 2330	DRAWING NO: 2330.ALDTUR.PF.04		REVISION: B



GROUND FLOOR PLAN

Scaled @ 1:100



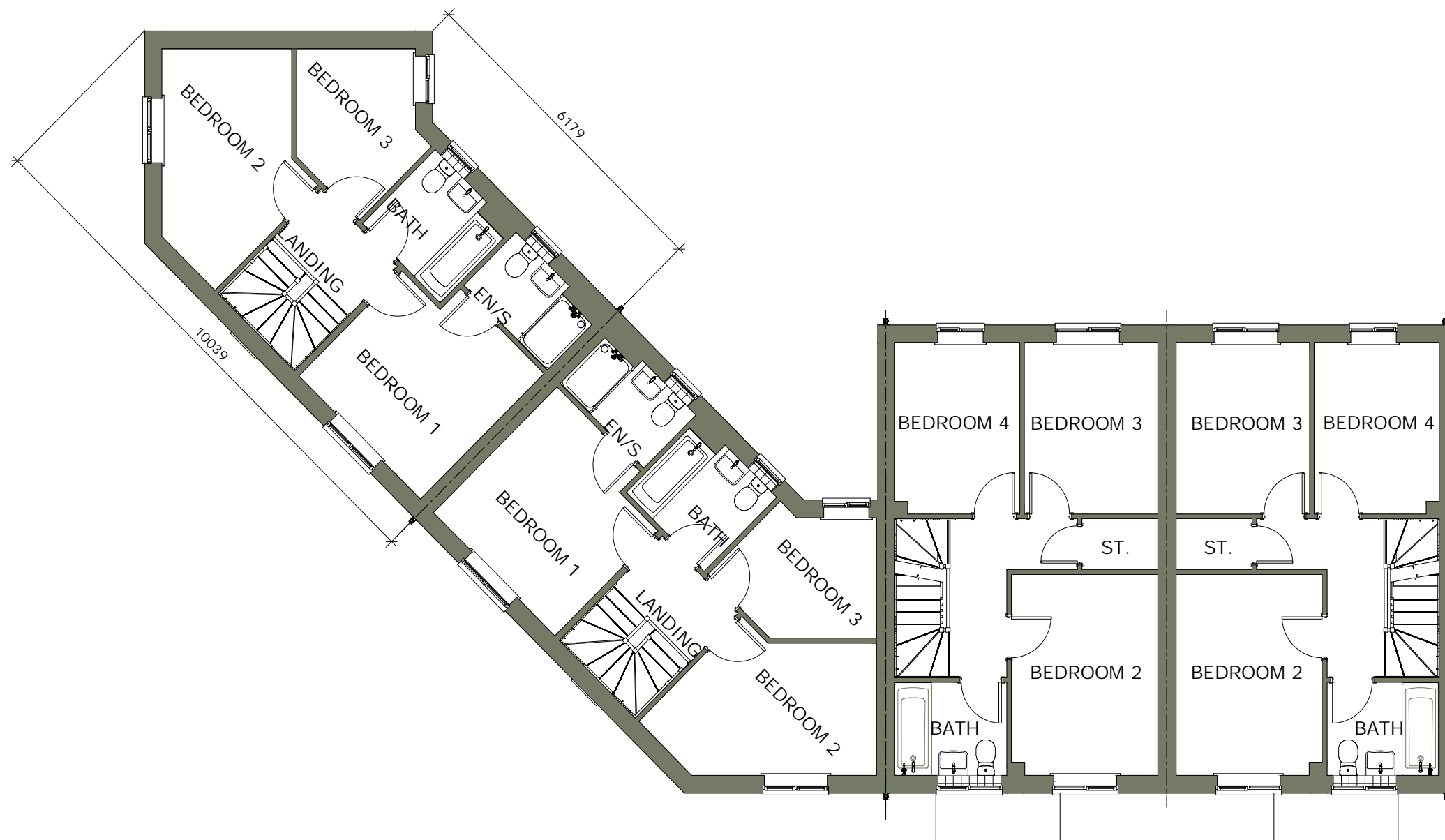
REV:	DESCRIPTION:	BY:	DATE:

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CLIENT: Countryside Partnerships		TITLE: Parcel F - Planning Drawings Aldridge / Turner 4 Block - Plans	
SITE: Leighton West Crewe	SCALE AT A3: 1:100	DATE: 05.12.23	CHECKED: SL
	PROJECT NO: 2330	DRAWN: DS	REVISION: --
		DRAWING NO: 2330.ALDTUR.PF.01	



FIRST FLOOR PLAN

Scaled @ 1:100



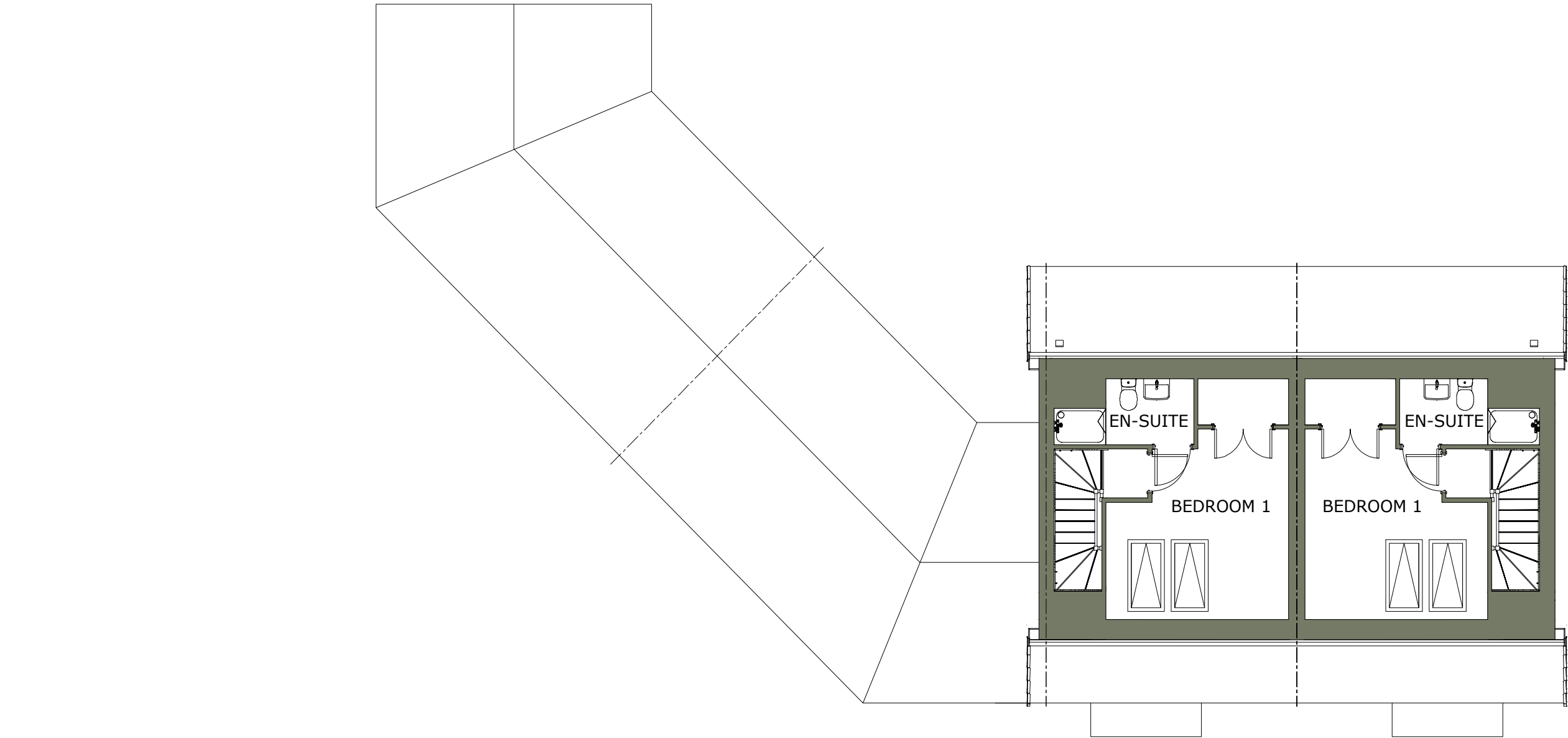
REV:	DESCRIPTION:	BY:	DATE:

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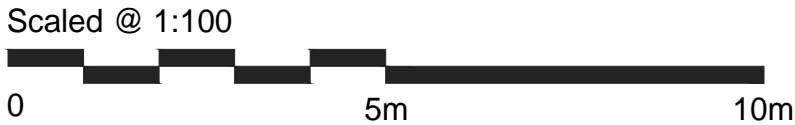
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CLIENT: Countryside Partnerships		TITLE: Parcel F - Planning Drawings Aldridge / Turner 4 Block - Plans	
SCALE AT A3: 1:100	DATE: 05.12.23	DRAWN: DS	CHECKED: SL
PROJECT NO: 2330	DRAWING NO: 2330.ALDTUR.PF.02	REVISION: --	
SITE: Leighton West Crewe			



SECOND FLOOR PLAN



REV: DESCRIPTION:		BY:	DATE:	<div><div>STEN ARCHITECTURE</div><div>STEN Architecture Ltd The Studio Harrison Street Wakefield WF1 1PS Tel: 01924 950985</div><div>Web: www.sten-architecture.co.uk Twitter: @STEN_arch Facebook: stenarchitectureltd Linkedin: Sten Architecture</div></div>		CLIENT: <div>Countryside Partnerships</div>		TITLE: Parcel F - Planning Drawings Aldridge / Turner 4 Block - Plans					
PROJECT NO: 2330		DRAWING NO: 2330.ALDTUR.PF.03		REVISION: --		SCALE AT A3: 1:100		DATE: 05.12.23		DRAWN: DS		CHECKED: SL	
SITE: Leighton West Crewe													

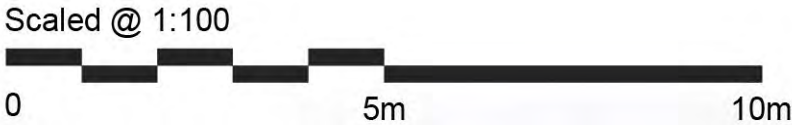


FRONT ELEVATION



REAR ELEVATION

* N.B. Landscaping shown to some elevations is a living green wall.



B	Communal entrances updated.	DS	24.04.24
A	Design updated for easy conversion.	DS	12.03.24
REV.	DESCRIPTION:	BY:	DATE:

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CLIENT:
Countryside Partnerships

SITE:
Leighton West
Crewe

TITLE: Parcel F - Planning Drawings Apartment Elevations			
SCALE AT A3: 1:100	DATE: 05.12.23	DRAWN: DS	CHECKED: SL
PROJECT NO: 2330	DRAWING NO: 2330.APT.PF.04	REVISION: B	

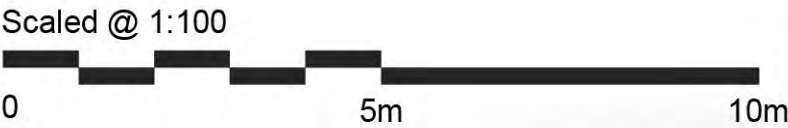


SIDE ELEVATION



SIDE ELEVATION

* N.B. Landscaping shown to some elevations is a living green wall.



B	Communal entrances updated.	DS	24.04.24
A	Design updated for easy conversion.	DS	12.03.24
REV:	DESCRIPTION:	BY:	DATE:

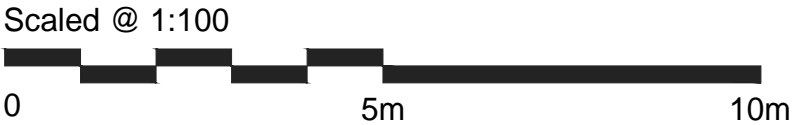
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CLIENT:	Countryside Partnerships			TITLE:	Parcel F - Planning Drawings Apartment Elevations		
SITE:	Leighton West Crewe			SCALE AT A3:	DATE:	DRAWN:	CHECKED:
				1:100	05.12.23	DS	SL
				PROJECT NO:	DRAWING NO:	REVISION:	
				2330	2330.APT.PF.05	B	



GROUND FLOOR PLAN



B	Communal entrances updated.	DS	24.04.24
A	Design updated for easy conversion.	DS	12.03.24
REV:	DESCRIPTION:	BY:	DATE:

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CLIENT: **Countryside Partnerships**

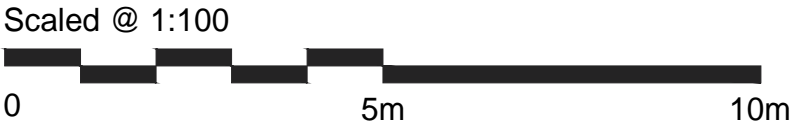
SITE: **Leighton West Crewe**

TITLE: **Parcel F - Planning Drawings
Apartment Plans**

SCALE AT A3: 1:100	DATE: 05.12.23	DRAWN: DS	CHECKED: SL
PROJECT NO: 2330	DRAWING NO: 2330.APT.PF.01	REVISION: B	



OPTIONAL GROUND FLOOR PLAN



REV:	DESCRIPTION:	BY:	DATE:

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CLIENT: **Countryside Partnerships**

SITE: **Leighton West Crewe**

TITLE: Parcel F - Planning Drawings Apartment Plans			
SCALE AT A3: 1:100	DATE: 24.04.24	DRAWN: DS	CHECKED: SL
PROJECT NO: 2330	DRAWING NO: 2330.APT.PF.011		REVISION: -



FIRST FLOOR PLAN

Scaled @ 1:100



A	Design updated for easy conversion.	DS	12.03.24
REV:	DESCRIPTION:	BY:	DATE:

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CLIENT:

Countryside Partnerships

TITLE:

Parcel F - Planning Drawings
Apartment Plans

SCALE AT A3:

1:100

DATE:

05.12.23

DRAWN:

DS

CHECKED:

SL

PROJECT NO:

2330

DRAWING NO:

2330.APT.PF.02

REVISION:

A

SITE:

Leighton West
Crewe



SECOND FLOOR PLAN



A	Design updated for easy conversion.	DS	12.03.24
REV:	DESCRIPTION:	BY:	DATE:

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CLIENT: **Countryside Partnerships**

SITE: **Leighton West Crewe**

TITLE: **Parcel F - Planning Drawings Apartment Plans**

SCALE AT A3:	DATE:	DRAWN:	CHECKED:
1:100	05.12.23	DS	SL
PROJECT NO:	DRAWING NO:	REVISION:	
2330	2330.APT.PF.03	A	

Application No: 22/2500N

Location: Land Off Minshull New Road And, FLOWERS LANE, LEIGHTON

Proposal: Reserved matters application for appearance, landscaping, layout and scale with respect to 200 dwellings on Parcel H - approved outline application 19/2178N.

Applicant: Bloor Homes and Vistry Partnerships Ltd

Expiry Date: 25-Oct-2022

SUMMARY

This application proposes the Reserved Matters for a Parcel of land, approved in outline and subject to a Section 106 legal agreement, under application reference 19/2178N. It is one of four separate applications being considered on this agenda.

This Parcel (H) makes up the northern most part of the site, with the site being split between Bloor Homes and Vistry Partnership as applicant's here. A number of changes have been made to the application since it was submitted, with the latest amendments being received at the time of writing this report.

Highways have raised no objections, subject to some points of detail being agreed by conditions and informatives. The PROW Team have no objections and the proposed footpath/cycleway provision is considered to be good.

The majority of the Ecology and Landscaping issues identified in this report have now been resolved, and although there are some outstanding matters to do with tree impacts, it is anticipated most of these can be addressed and Members will be updated on this matter.

Extensive discussions have been had regarding urban design and the Council's Urban Design Officer now feels the proposals have improved significantly and are therefore supported in design terms.

The Council's Flood Risk (LLFA) team have similarly been involved in detailed discussions with the applicant and detailed replies to matters raised have been submitted by the applicant. The LLFA's latest comments were awaited at the time of writing this report, but the main issues have now appear to have been addressed, and if necessary outstanding matters can be conditioned.

There are no objections on the grounds of affordable housing, public open space, residential standards, or environmental matters.

RECOMMENDATION

Approve with conditions

SITE DESCRIPTION

This application relates to a sizable site north of Flowers Lane in North West Crewe. The site adjoins a development site to the east, but with open fields to the north. The site is referred to as Parcel H, the most northerly of a series of sites subject to consideration.

There are hedgerows with some trees to the site boundaries, and some scattered trees and remnants of hedgerows within the sites themselves, however they are limited in number and the site is generally open in character.

There are no conservation areas on or adjoining the site, and there are no listed buildings on or adjacent to the site.

A public footpath, Leighton FP2 runs east west along the southern boundary of the site.

The sites falls in Flood Zone 1 – Least risk of flooding.

PROPOSAL

The application title reads:

“Reserved matters application for appearance, landscaping, layout and scale with respect to 200 dwellings on Parcel H - approved outline application 19/2178N.”

Reserved Matters approval is sought for Appearance, Landscaping, Layout & Scale.

Not forming part of this application, but shown for illustrative purposes, is the primary school for this allocation together with the playing field facilities, and public open space – which forms part of the green infrastructure application also considered on this agenda, with facilities described below, which extend along the northern area of the site.

The site makes up the north western part of LPS5 - the remainder of the allocation to the south west having received outline and reserved matters consent, and development is well underway.

The main elements of the proposed development consist of:

Vehicular access is proposed from a new roundabout on Flowers Lane, being built as part of the North West Crewe Package referenced below, which would be shared with Leighton Hospital.. An internal link to the adjacent site is proposed on the eastern boundary. Additional pedestrian access is proposed at various points along the public footpath to the south.

The site is to be split in two between two developers, with Bloor having half (100 units), and Vistry the other half (100 units). The proposed mix is as follows:

- 180 Private dwellings in 2,3 & 4 bed units
- 20 Affordable dwellings in 1, 2 7 3 bed units

Number of Beds	Number of Units	%
Private Dwellings		
2	50	28%
3	91	50%
4	39	22%
Affordable Dwellings		
1	2	10%
2	14	70%
3	4	20%

Table 1: Parcel H Schedule of Accommodation

Finally, a LAP is proposed in the centre of the site, and on land to the north, adjacent to the school site, are a proposed NEAP, MUGA, Teen facilities and areas of open space

An Environmental Impact Assessment (EIA) was submitted with the outline application.

RELEVANT PLANNING HISTORY

Outline planning permission was granted under:

19/2178N Outline planning approval for the development of up to 850 residential units (Use Class C3), land reserved for new primary school, a local centre (Use Class A1-A4, AA, B1a, C3 and D1) and associated infrastructure and open space. Land off Minshull New Road and FLOWERS LANE, LEIGHTON APPROVED Nov 2021

In addition, planning approval was granted for a series of Highway works in the immediate vicinity:

18/6118N A proposed series of highway infrastructure measures and associated works, in the Leighton area of Crewe, and known as the North West Crewe Package - Land Between Flowers Lane Minshull New Road The A530 Middlewich Road And North Of, PYMS LANE, CREWE APPROVED March 2019

Finally, this is one of four reserved matters applications submitted for Leighton West. All are on this agenda. The others are:

22/2476N Reserved Matters application for approval of appearance, landscaping, layout and scale following Outline approval 19/2178N for erection of 304 dwellings on Parcels A and B Land Off Minshull New Road And, FLOWERS LANE, LEIGHTON

22/2499N Reserved matters application for appearance, landscaping, layout and scale with respect to 304 dwellings on Parcels C, D, E, F and G. An Environmental Statement was submitted at the time of the outline application. Land Off Minshull New Road And, FLOWERS LANE, LEIGHTON

22/3228N Reserved matters application for appearance, landscaping, layout and scale with respect to green infrastructure and open space across the outline application area - approved outline application 19/2178N. Land Off Minshull New Road And, FLOWERS LANE, LEIGHTON

Other approvals under LPS 5, immediately to the south east include:

16/2373N Outline application for the construction of up to 400 dwellings with garaging; parking; public open space; landscaping; new vehicle and pedestrian accesses; highway works, foul and surface water drainage infrastructure and all ancillary works. - Land At, Flowers Lane, Leighton APPROVED January 2018

20/3210N Reserved matters approval sought for access, appearance, landscaping, layout and scale. following outline permission for the construction of up to 400 dwellings with garaging; parking; public open space; landscaping; new vehicle and pedestrian accesses; highway works, foul and surface water drainage infrastructure and all ancillary works. Land At, FLOWERS LANE, LEIGHTON APPROVED July 2021

This development is now on site.

Finally, there are numerous approvals at Leighton Hospital, the most relevant recent approval being a car park on the north eastern side fronting Flowers Lane.

POLICIES

Cheshire East Local Plan Strategy (LPS) – 2010-2030

PG1 – Development Strategy
PG6 – Open Countryside
SD1 - Sustainable Development in Cheshire East
SD2 - Sustainable Development Principles
IN1 – Infrastructure
IN2 – Developer Contributions
SC1 – Leisure and recreation
Sc2 – Indoor and outdoor recreation
SE 1 - Design
SE 2 - Efficient Use of Land
SE3 – Biodiversity and Geodiversity
SE 4 - The Landscape
SE 5 - Trees, Hedgerows and Woodland
SE 6 - Green Infrastructure
SE 13 - Flood Risk and Water Management
CO1 – Sustainable Travel and Transportation

LPS4 – Leighton West

LPS5 – Leighton

Site Allocations and Development Policies Document (SADPD) 2022

GEN 1 - Design principles
GEN 4 - Recovery of forward-funded infrastructure costs
ENV 1 - Ecological network
ENV 2 - Ecological implementation
ENV 5 – Landscaping
ENV 6 - Trees, hedgerows and woodland implementation
ENV 7 - Climate change
ENV 14 - Light pollution
ENV 16 - Surface water management and flood risk
HOU 1 - Housing mix
HOU 8 - Space, accessibility and wheelchair housing standards
HOU 13 - Residential standards
HOU 14 - Housing density
HOU 15 - Housing delivery
INF 1 - Cycleways, bridleways and footpaths
INF 3 - Highway safety and access
INF 6 - Protection of existing and proposed infrastructure
INF 9 – Utilities
REC 3 - Open space implementation
REC 5 - Community facilities

Neighbourhood Plans:

Crewe has not made any progress towards making a Neighbourhood Plan, and Minshull Vernon is at Regulation 7 Stage: Designated Neighbourhood Area, but at this stage can be given very little weight.

Other Material Considerations

The National Planning Policy Framework

CONSULTATIONS (External to Planning)

United Utilities: Whilst originally objecting to the application, they have now confirmed the applicant's more recent submissions have addressed their concerns and they raise no objections subject to informatives concerning asset protection.

SP Energy Networks: No objections are raised, but they remind the applicant of their health & safety responsibilities around the significant electricity infrastructure which passes through the site, and the safe working practices that should be employed.

CEC Head of Strategic Infrastructure: No objections subject to conditions and informatives.

CEC Housing: Whilst originally raising objections to the application, as there was uncertainty about the affordable numbers and mix, following the submission of additional supporting information Housing have now confirmed they have no objections to the application.

CEC Public Rights of Way: In their original comments they raised a number of issues with the proposals, however following submission of revised plans these issues have largely been addressed and they now raise no objections.

CEC Environmental Health: No objections are made, subject to reminding the applicant of their responsibilities to discharge the conditions under the outline approval relating to amenity and air quality, and the contaminated land team recommends a further condition. Informatives are recommended.

CEC Flood Risk Manager: Extensive discussions have taken place and updated comments are awaited at the time of writing the report.

ANSA: Now that the range of facilities has been clarified across the site they raise no objections, however they recommend the detailed design of facilities is conditioned.

Archaeology: No objections are made but a condition is recommended as set out in the report below.

VIEWS OF THE TOWN/PARISH COUNCILS

Minshull Vernon and District Parish Council: No comments received.

Crewe Town Council: “The committee seeks that the planning authority ensures the drainage strategy is robust and provides long term assurance”.

OTHER REPRESENTATIONS

No representations have been received to this application.

OFFICER APPRAISAL

Principal of Development

The site is allocated in Local Plan policy LPS 4 and has the benefit of outline planning permission as set out above. The outline set out the requirements for the site, and the Section 106 contributions/requirements. The policy reads:

Site LPS 5 Leighton, Crewe

This site lies adjacent to the Strategic Site of Leighton West. The site lies in close proximity to both Leighton Hospital and Bentley Motors; its development will result in the expansion of the new sustainable neighbourhood at Leighton West. This will provide opportunities for people to live near to the key automotive hub, to be located at the southern end of the Leighton West site and in close proximity to Bentley Motors. The development of the Leighton West Strategic Site over the Local Plan Strategy period will be achieved through:

1. The delivery of around 500 homes (at a variety of densities). The design, density and scale of the development should reflect the fact that the site lies in a transitional location between the higher density urban area and the rural area. The surrounding development is predominantly suburban and the development of the site should reflect this. The development of the site will be masterplan-led, including a design code, which will consider its location, constraints and opportunities;

2. Further road improvements to upgrade access to Leighton Hospital for emergency vehicles and suitable footpath and cycle lanes;
3. Key worker housing to be provided, for the employees of Leighton Hospital;
4. Site to be designed to complement the allocated site at Leighton West; and
5. Incorporation of green infrastructure, including:
 - i. A linear green corridor through the site, including the land below and adjacent to the pylons;
 - ii. Allotments;
 - iii. Open space including formal sports pitches, multi-use games area; outdoor gym and equipped children's play space.

The site, subject to this application consists of the northern half of LPS5 (the southern half already benefiting from outline planning permission as referenced above) and the northern half of LPS 4, a further part of LPS4 is subject to a separate application also on this agenda. Both sites share a joint masterplan and together make up a significant part of both allocations, but with areas to the south adjoining Leighton Brook and Bentley Motors being excluded.

As the application proposes the erection of dwellings which in principle accords with the Local Plan allocation and the parameters set in the outline approval, it is considered the application is in principle acceptable. The (relevant) site specific requirements are explored below.

Highway Implications

This site forms part of the local plan allocation LPS5 and already has outline approval where access was determined including the wider highways impact and any mitigation required. This current proposal is the reserved matters for the parcel of development north of Flowers Ln which will share a roundabout access with the hospital car park.

The main spine road through the site runs from the roundabout to the approved site to the east and will have sufficient width to cater for through traffic. The adjacent streets off the spine road will be built to adoptable standards and adequate parking provision it to be provided throughout. The spine road through the site is relatively straight and bus friendly raised tables should be installed to assist in managing vehicle speeds, and this can be conditioned.

There is to be a primary school to the north of the spine road but it does not form part of this application and details are not yet available.

There will be pedestrian accesses to the playing fields to the north and another to the adjacent site on the south-eastern corner via the existing PROW. Some additional pedestrian connections should be added taking into consideration desire lines, this would require a minor amendment to the plans and can be conditioned.

There are a lot of trees along the spine road and streets off it which will hinder access visibilities. Whilst trees in the visibility splay has shown not to raise a safety concern, and trees within the splays are acceptable in principle, there are occasions where there up to 4 or 5 trees in a single splay which is too many. For the most part the accesses will be acceptable otherwise only minor amendments will be required and this can also be conditioned.

Other than a few minor amendments the layout is acceptable and no objection is raised subject to conditions and informatives.

Public Rights of Way/Cycle routes

Commenting on the original submission, the PROW Team noted that Leighton FP 2 appears to be largely accommodated within a 3 metre shared cycleway with a resin bound surface. The alignment of the path in the southwest corner is still an area of concern as the definitive route must have provision to meet the Flowers Lane and remain unobstructed. At the south east corner they highlighted the issue of the adjoining path in the neighbouring development having been the subject of a diversion order. This Order has now been confirmed although not certified. It is not clear how this connection will be made and there are also concerns as the adjoining path is 2 metre wide, tarmac surfaced footpath only according to the legal order: 102 (2) (cheshireeast.gov.uk), therefore a 3 metre shared route in Parcel H might not be appropriate. The applicant should assure themselves that this connection is suitable for cyclist use and also consider the responsibility for the boundary furniture which is currently a kissing gate.

Following discussions with the applicant the footpath will be re-designed to match that on the adjacent site (2m wide) which the PROW have agreed is the sensible option.

In terms of walking and cycle access there are two main elements of provision. Firstly, there are the footpath/cycle routes which run alongside the new roads which make up the new highway network in the area, and provide links south, through the other elements of this overall development, and towards Bentley Motors and Nantwich, and east towards Crewe Town Centre.

Reenforcing these highway links are those now proposed through this application which link the site to the PROW which runs along the southern boundary and to the adjacent development(s). Highways are looking for some of the detail of these connections to be clarified but this can be conditioned.

The overall provision is considered to be good and should encourage residents to walk and cycle, in line with policy objectives.

Landscape

The Council's Landscape Architect comments that in Bloor Parcel H some of the boundary landscaping has areas of little tree planting, a few more trees would remedy this. Assurance is sought that Parcel H and G have integrated planting schemes along shared road boundaries, i.e. the same landscape concept, it would be good therefore to integrate the Vistry planting into the Parcel H plans to demonstrate the planting consistency.

The applicant has looked at these matters and has now submitted a Landscape Masterplan to better set the context. More tree planting is now proposed in the housing areas which addresses the principal concerns of the Landscape Architect.

Trees

An amended Arboricultural Impact Assessment (AIA) and an updated Arboricultural Method Statement (AMS) has been submitted further to earlier forestry comments. Clarification has been provided regards several of the issues raised as requiring more detail and explanation.

It is understood that tree and hedgerow losses which have occurred since the preliminary survey was carried out at Outline have arisen because of Highway Infrastructure projects and the supporting plans are now annotated to confirm where this is believed to be the case. The TPO trees (Group G7) of the

survey are shown for removal as this route into the site is reported to be an agreed bus link into the site from the neighbouring development area and already agreed.

The updated AIA and AMS has now been broken down by phase of development and makes provision to show all trees including those presently shown for removal shown by tree quality category colour with RPAs, so that impacts can be better understood. Some explanatory notes have been provided regards the pre agreed loss of TPO'd trees and those trees already removed for the highway's infrastructure projects.

The AIA indicates that of the trees surveyed across the wider site that 2 individual and 1 group of high quality A Category tree, 3 individual moderate quality B Category trees, 1 individual and 1 group of low-quality C Category trees and 1 poor quality U Category tree as well as a combined total of 52.2 linear metres from 4 hedgerows are proposed for removal to accommodate the proposals associated within Parcel H of the development. An updated AMS (Version 3) has also been provided.

The submitted Assessment of Important Hedgerows considers the whole of the site and has not been broken down to impacts on the submitted applications. Twelve hedgerows (H1-H12) have been identified as important in accordance with criteria 5a of the Hedgerow Regulations.

5. The hedgerow:

a) is recorded in a document held at a Record Office as an integral part of a field system pre-dating the Inclosure Acts

Hedgerow 1 is also importance in accordance criteria 1.

1. Any hedgerow that marks the boundary or part of the boundary, of at least one historic parish or township; and for this purpose, "historic" means existing before 1850.

The assessment confirms that sections of hedgerow H2, H5, H6, H7 AND H8B0 are to be removed as part of proposed development for Northwest Crewe Package and includes the removal of sections for Highway works. Additional removal of H3 is required for a bus route and access. The overall development will arise in the partial loss of hedgerows which form a 19th Century field system but most of the hedgerows are shown for retention where possible and the report has concluded that the overall harm is; 'less than substantial'. Mitigation is proposed of 2,300m of new hedgerows. As hedgerows have been found to be 'Important' under the above referenced criteria of the Regulations, the loss of the hedgerows to accommodate the development is a material consideration in the determination of the application although it appears that sufficient compensatory planting is being offered across the whole development area.

Having appraised the amended detail and proposals identified it is unclear why high quality and moderate quality trees; namely T37, and trees in groups G1 and G4 could not also be retained.

Initial observations are as follows; separation between plots proposed along the eastern boundary of Parcel H and mature, moderate quality trees in groups G5, G6 and G8 is poor in relation to plots 44-48, 53-54 and, 57-58 and presents concerns in terms of shading and dominance to the dwellings and outside amenity space. BS 5837:2012 Trees in relation to design, demolition and construction – Recommendations at section 5.2 - Constraints posed by existing trees, and 5.3 - Proximity of structures to trees, acknowledges the importance of design and the relationship of trees with new development. The standard places importance on buildings and structures being positioned in such a way that they will not dominate a property or its outdoor space in such a way as to cause apprehension which could result

in pressure to prune or remove trees in the future, and the relationship of trees with the plots referenced should be re considered.

Further detail should be provided as to why high-quality group G1 cannot be retained as the trees appear adjacent to a pedestrian path near an open space feature. Group G4 shown for removal also stands within open space although close to a path. T37 stands within a retained boundary hedgerow near other trees with similar separation from the proposed road to the south but is also shown for removal. The RPA's have not been indicated for any of these trees, but notwithstanding this it is disappointing that the retention of existing landscape features does not appear to have been a priority with the layout of Parcel H and explanation and further clarification should be provided as to why minor amends cannot be made to secure the retention of more trees within the scheme.

The AMS as submitted does include a tree protection plan, make provision for a methodology for working in relation to retained trees, and identifies areas where special measures would be implemented in respect of minor root pruning and construction of engineer designed surfaces. Notwithstanding this, considering the tree losses which have already been carried out because of the Highway improvements and further losses proposed arising from this layout, opportunities exist to retain further trees. Separation between trees G5, G6 and G8 and the closest plots should be increased and minor amendments to the landscape layout considered in respect of pedestrian pathways, open space and trees shown for removal to see if additional moderate quality trees such as G1, G4 and T137 could be retained and to provide more confidence that the most significant trees on the site will have a sustainable relationship with residential property in the longer term.

For completeness in the assessment and determination of this planning application, as hedge loss is involved it is considered the hedgerows should be assessed against all criteria in the Hedgerow Regulations 1997 in order to ascertain if they qualify as 'Important'. The Regulations require assessment on various criteria including ecological and historic value. Should any hedgerow shown for removal be found to be 'Important' under any of the criteria in the Regulations, this would be a material consideration in the determination of the application. Hedgerows are also a habitat subject of a Biodiversity Action Plan.

The applicant has been looking at the impacts on trees identified above as part of the revised plans, as whilst the Council's Tree Officer has not formally objected to the application on the basis of tree impacts, at least some of these impacts would seem to be avoidable. At the time of writing this report the applicant was discussing amendments with the Council's Tree Officer which would allow for the retention/better protection of more trees, and Members will be updated on this matter in an Update Report.

Ecology

A number of conditions were attached to the outline consent at this site.

Condition 9 Ecological Monitoring Strategy:

Monitoring proposals as required by this condition are included in the submitted Habitat Management Plan TEP Version 3.0 dated 10.4.24.

Condition 11 Detailed design of attenuation ponds:

No detailed designs of attenuation ponds have been submitted with this reserved matters application. I understand no attenuation ponds are proposed as part of this reserved matters application.

Condition 12 Updated hedgerow impact assessment, mitigation and compensation strategy:

The submitted biodiversity assessment (January 2024) however advises that 2.6km of hedgerow would be lost across the entire outline site under the various reserved matters applications, with 2.17km of new hedgerow planting being provided. The originally submitted BNG metric concluded that the reserved matters applications would result in a net loss of 10.41% in respect of hedgerow biodiversity.

It was advised that the development proposals must ensure that the extent of hedgerow retained as part of the development is maximised and that any unavoidable losses of hedgerow are adequately compensated for by appropriate native species hedgerow planting. A greater length of replacement planting to that lost will be required to fully address the loss of biodiversity from hedgerow removal. Ornamental hedgerow planning is not acceptable as compensation for the loss of native species hedgerow.

Following the submission of further information to clarify the impacts of this development as opposed to losses for the road scheme – which appears to have caused the confusion, the Council's Ecologist comments:

“The proposed reserved matters application combined will result in the loss of 0.38km of hedgerow, including lengths of hedgerow considered to be ‘Important’ under the Hedgerow Regulation. The BNG metric has been undertaken on the basis of 2.17km of new hedgerow planting being provided on site. This is based on PGLA Landscape Masterplan Rev 11.

The proposed hedgerow planting would result in a net gain of hedgerows of 30.57%. Therefore, if the loss of the existing hedgerows is considered unavoidable sufficient planting would be provided to compensate for that lost. This would however be dependent upon the reserved matters application being brought forward in accordance with the landscape master plan.”

This masterplan would, should planning permission be granted, form one of the approved plans.

Condition 25 Updated badger and barn owl survey and mitigation strategy:

No information as required by this condition has been submitted in support of this reserved matters application.

Condition 26 Details of any proposed lighting. The lighting strategy should reflect the Bat Conservation Trust Guidance Note:

Lighting plans have been submitted in support of this application. No information, such as the 1 lux lighting contour, as required by this condition has however been submitted. It is advised that the submitted lighting plan must be revised to include the 1 lux lighting contour and the lighting scheme must seek to avoid any light spill of greater than 1 lux on any suitable bat foraging or commuting habitat, such as boundary hedgerows or trees.

Condition 27 25 year habitat management plan:

An acceptable Management plan has been submitted

Condition 28 The first reserved matters application shall be supported by a strategy for the incorporation of features to enhance the biodiversity value of the proposed development:

A Biodiversity Impact Assessment which covers the full extent of the outline planning consent subject to the various current reserved matters applications was previously submitted in support of this reserved matters application. A revised BNG metric dated 22/1/24 has also now been submitted. The metric includes measurements for the areas of various habitats to be provided on site. The area of species rich

grassland provide don site is less than that required by condition 28 as is the extent of hedgerow planting. The area of cornfield annuals and wet grassland is however greater than required by condition 28.

The assessment concludes that the proposed development (based on all of the reserved matters applications) would deliver a net gain for area based habitats of 12%, but a net loss of -10.41% of hedgerow biodiversity.

Proposals for the provision of bird and bat boxes and gaps in garden fences have been submitted as required by this condition. It must however be ensured that bat/bird boxes are proposed to be attached to 30% of consented dwellings in accordance with the Cheshire East Design Guide.

Detailed method statements for the creation of species rich grassland and cornfield annual meadow habitats are outstanding. These must be submitted prior to the discharge of this condition.

Additional conditions

If reserved matters consent is granted a condition is required to safeguard nesting Birds.

Whilst ideally all matters would have been resolved now, outstanding matters relating to the discharge of conditions highlighted above, will be picked up in their subsequent discharge.

Urban Design

The development has been assessed as a whole (considering the four Reserved Matters (RM) applications collectively) using the Building for a Healthy Life (BHL) considerations. Performance is summarised in the table below. A more detailed commentary in relation to each of the considerations has also been provided, but not included within this report.

Integrating into the Neighbourhood				Distinctive Places				Streets for All			
1 Natural Connections	2 Walking, cycling public transport	3 Facilities and services	4 Homes for everyone	5 Making most of what's there	6 Memorable character	7 Well defined streets/spaces	8 Easy to find your way around	9 Healthy streets	10 Cycle and car parking	11 Green and blue infrastructure	12 Back of pavement, front of home
G	G	G	G	G	A	G	G	A	A	A	A

Summary/conclusions

This is a large and complex scheme compounded by the submission of several separate ARM applications, prepared jointly by the two developers. Significant input has been necessary to improve the quality of the originally submitted proposals. Key aspects are:

- Securing a more responsive, forward-thinking approach to the design of the local centre and other parcels within proximity to Leighton Hospital.
- Ensuring the residential component of the local centre acts as a positive lead for the design and development of the future commercial area including securing detailed coding to help guide the design of the commercial area and associated public realm.
- Developing a character area approach for the remainder of the scheme to ensure a degree of continuity and consistency between developers but also some localised individuality (albeit this could have been taken a little further).
- Ensuring there is a clear street hierarchy, with tree lined avenues within the larger parcels and stronger, more formal gateways into smaller ones, with a supplementary hierarchy of streets and spaces, appropriately detailed and surfaced to suit their function.

- Ensuring appropriate levels of landscaping along the edges of development parcels.
- Inclusion of feature spaces within layouts to help reinforce the social function of streets, better handle concentrations of parking and to create points of incident and interest.
- Seeking to integrate existing landscape features within the layout whilst accepting the constraints imposed in certain locations and the need to connect to adjoining developments and the approved highway improvement scheme (Northwest Crewe package currently under construction).
- Ensuring the inclusion of blue infrastructure as a positive element of the place creation adhering to principles within the recently adopted SuDS Design Guide SPD, with a series of basins, swales and ditches for the outlying development areas and within the main central area of POS, and a more urban approach for the local centre, including living surfaces to buildings and structures, rain gardens and channels/rills within streets and spaces.
- Improved connectivity within the development and to the surrounding area with integrated public rights of way, combined footpath/cycleway routes and stronger interrelationship and connectivity to, within and along the central main POS area defined by the pylon route.
- Enhancements to the strategic approach to landscape design, including reflecting and building upon the character of the spaces, integrating existing landscape features, provision of varied play and leisure opportunity and inclusion of local food growing, including a central allotment area, orchard and on the doorstep, more informal, food growing within and adjacent to housing.

Whilst in certain respects the design response could have gone further/been more creative, the work invested has enhanced the scheme considerably from the initial submissions and will lead to a better quality and more cohesive development that responds more favourably to policy LPS4 (Parcels A-G) and LPS5 (Parcel H) of the CELPS, local design policy and guidance and the ambitions of the spatial design code submitted with 19/2178N. The proposals are therefore supported in design terms.

Residential standards

Whilst the development of parcel H is in relatively close proximity to the adjacent housing development – which is now on site, there is a good stand-off to the boundary from most properties, and even where there is a closer relationship to the north, the required separation distances are met, helped by mature hedgerows and trees.

Within the development parcels themselves the majority of the properties meet or exceed the required separation distances, and in the few cases where they don't the properties are slightly off set from one another or there are good urban design reasons for it.

Environmental Impacts

Environmental Protection have confirmed they have no objections to the application with regards to general amenity (including noise) and air quality subject to satisfactory discharge of the relevant conditions on the outline and informatives. The contaminated land team comment that they have no objection to the application subject to the following comments with regard to contaminated land:

- RoC Consulting Phase 1 Desktop Study was submitted in support of the outline application.
- The report identifies a number of potential pollutant linkages that require further investigation. The report recommends that a Phase II investigation is conducted at the site.

- A rigorous monitoring-based ground gas risk assessment will be required to assess the potential impact of ground-gas migration from the landfills adjacent south.

As such, and in accordance with the NPPF, the Contaminated Team recommends that that conditions, reasons and notes be attached should planning permission be granted.

Flood Risk/Drainage

Extensive discussions have taken place with the applicant's drainage engineers, with matters of relating to drainage calculations, finished floor levels, management arrangements and details of a pumping station being discussed. The applicant has submitted detailed replies to all matters raised, but at the time of writing this report the Flood Risk Manager's updated comments are awaited. It is considered that if there are any outstanding matters raised, they could be conditioned.

Public Open space

The public open space provision was set out at the outline stage and captured by the Section 106 Agreement. Overall, the site includes the following facilities:

- Multi Use Games Area (MUGA)
- Teen Shelter & Play
- Neighbourhood Equipped Area of Play (NEAP)
- An orchard
- Allotments
- Local Equipped Areas of Play (LEAP)
- Local Areas of Play (LAP) - 6 No.
- Areas of outdoor gym equipment
- Areas of incidental open space

Whilst many of these facilities are included with the common areas of green infrastructure which runs through the centre of the site, and along the northern boundary some are located within individual parcels where it was deemed preferable to give easier access for residents.

As noted above, for parcel H a LAP is proposed within the housing area, but to the north, west of the school site, effectively forming the outer edge of the allocated sites being considered here, are a proposed Multi Use Games Area (MUGA) a Teen Shelter & Play Facility and a Neighbourhood Equipped Area of Play (NEAP)

Due to the way the information was presented, ANSA where initially unclear what provision was being made, however following the submission of a Landscape Masterplan which sets it out more clearly they have confirmed they have no objections to the provision as submitted. There are however detailed design issues with the individual areas of provision, so ANSA have asked for the detailed design of each of the items listed above (incidental open space excluded) to be subject of a condition.

Affordable Housing

The Section 106 Agreement attached to the outline approval for the site required 10% of the houses on site to be affordable – based on a viability appraisal which was independently assessed at the time. The policy compliant 35% Shared Ownership/65% Affordable Rent was also required.

Now that the applicant has clarified the provision, by the submission of an updated Affordable Housing Statement which shows the number of units in each tenure, the number of beds and room sizes, and how they are laid out on site Housing have confirmed they are happy with the proposals. In parcel H 20 units are shown 10% of the 200 units proposed. All the properties meet the NDSS.

It is now considered that the affordable housing provision is acceptable.

Archaeology

Commenting on the applications as a whole, the Archaeological Unit write that this application is for a significant development on the land off Minshull New Road and Flowers Lane, on the outskirts of Crewe. This substantial residential and landscaping development will have a dramatic impact on the landscape and any below ground remains which may be present.

After reviewing the extensive supporting documentation for this application, which included a Heritage Statement authored by Orion Heritage, and reviewing the historical information held on Cheshire Historic Environment Record. It has become clear that there are archaeological considerations which will need to be addressed for this development.

Previous consultations provided by APAS have offered some insight into the potential archaeological remains that this proposed development will impact. A previous officer offered advice relating to application 18/6118N in 2018 noting that there are two township boundaries within the proposed development area. Furthermore the previous officer noted that the presence of the WWII crash site within the proposed development area. The officer recommended archaeological mitigation to address the township boundaries.

Another officer offered comments in 2017 in relation to a pre application consultation, and identified the presence of tithe buildings within the East of the application area, relating to Finger Post Farm. The officer at the time recommended a programme of archaeological mitigation to address these tithe buildings.

The historical statement report supplied by Orion Archaeology indicates in section 10.79 that there are no substantial archaeological remains and therefore there “No archaeological mitigation is warranted” The report does note the presence of the WWII crash site, but neglects to mention the two township boundaries, the tithe buildings in association with Finger Post Farm, or the tithe buildings located to the West of the development area. Subsequently, there clearly is a requirement for archaeological mitigation in order to address and record these tithe buildings and township boundaries.

The area of the WWII crash site, “The Wellington X3547” was recorded as crashed within the study area on 20th of March 1943. Any work in the vicinity of the crash site will require a Ministry of Defence license. There has been a previous license issued for this site, relating to the construction of the spine road, however, this has since expired and the applicant will need to apply for a new license in relation to this current proposed development. It is advised that this could form part of the program of archaeological mitigation and the license obtained on behalf of the applicant by the contract archaeological unit.

Given the presence of the tithe buildings, township boundaries and the crash site, it is clear that despite the comments in 10.79 of the Heritage Statement, there is a requirement for archaeological mitigation for this proposed development. This mitigation should include the following:

- Formal section across the northern Township boundary,
- Strip and Map of the tithe buildings associated with Finer Port Farm
- Strip and Map of the tithe buildings located in the West of the development area
- A License obtained from the Ministry of Defence

These works can be undertaken by an archaeological contractor and may be secured by condition.

CONCLUSIONS

This application proposes the Reserved Matters for a Parcel of land, approved in outline and subject to a Section 106 legal agreement, under application reference 19/2178N. It is one of four separate applications being considered on this agenda.

This Parcel (H) makes up the northern most part of the site, with the site being split between Bloor Homes and Vistry Partnership as applicant's here. A number of changes have been made to the application since it was submitted, with the latest amendments being received at the time of writing this report.

Highways have raised no objections, subject to some points of detail being agreed by conditions and informatives. The PROW Team have no objections and the proposed footpath/cycleway provision is considered to be good.

The majority of the Ecology and Landscaping issues identified in this report have now been resolved, and although there are some outstanding matters to do with tree impacts, it is anticipated most of these can be addressed and Members will be updated on this matter.

Extensive discussions have been had regarding urban design and the Council's Urban Design Officer now feels the proposals have improved significantly and are therefore supported in design terms.

The Council's Flood Risk (LLFA) team have similarly been involved in detailed discussions with the applicant and detailed replies to matters raised have been submitted by the applicant. The LLFA's latest comments were awaited at the time of writing this report, but the main issues have now appear to have been addressed, and if necessary outstanding matters can be conditioned.

There are no objections on the grounds of affordable housing, public open space, residential standards, or environmental matters.

RECOMMENDATION

Approve subject to the following conditions;

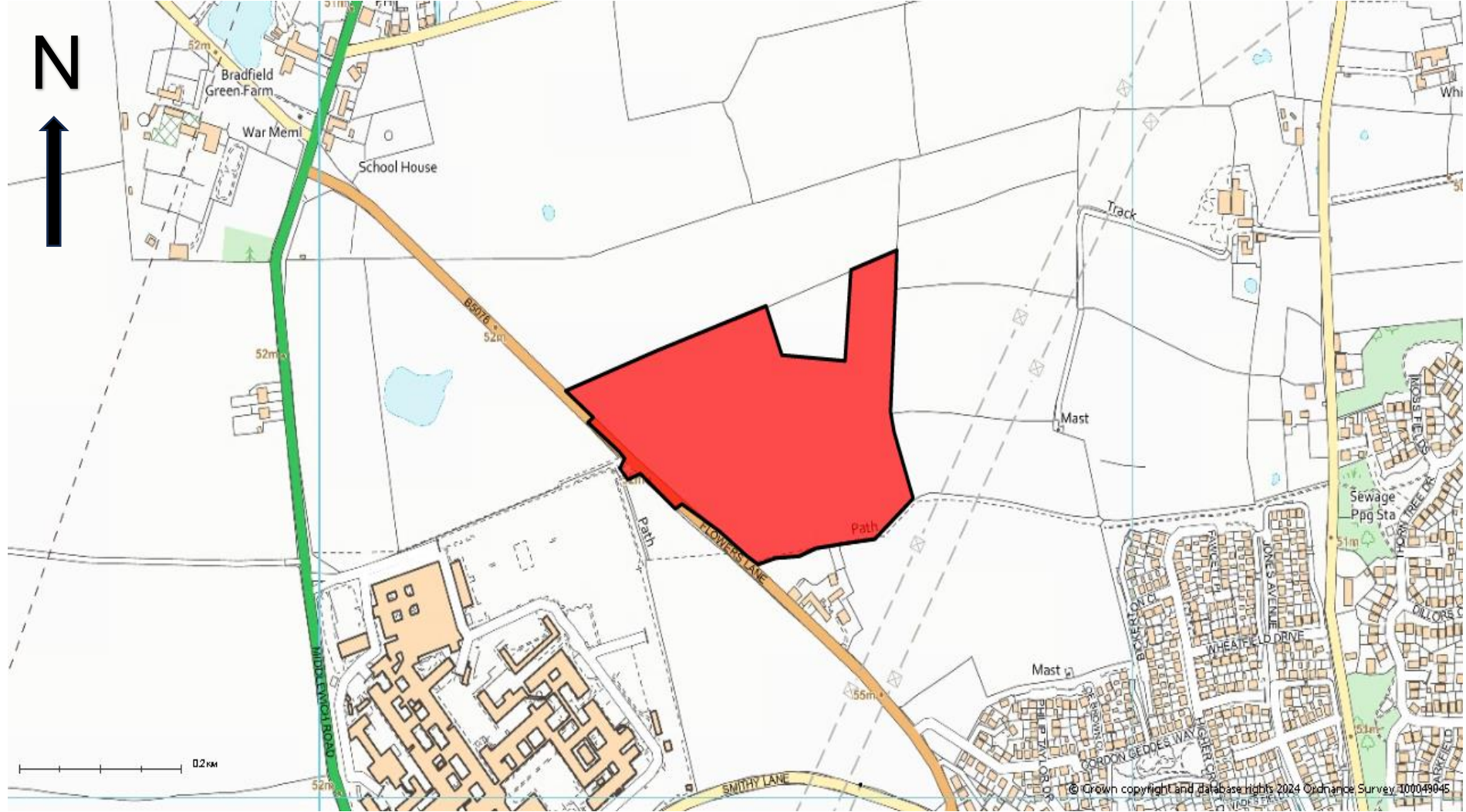
1. Approved plans
2. Bird nesting
3. Submission of a Phase II ground investigation and risk assessment
4. Submission of a Verification Report
5. Soils testing
6. Unexpected contamination
7. Archaeological mitigation

8. Detailed design of the individual elements of the play areas(LAP & NEAP)/Teen facilities/MUGA.
9. Detailed landscaping to be submitted and approved.
10. A plan showing raised tables on the spine road should be submitted and approved.
11. A plan showing improved pedestrian connections should be submitted and approved.
12. A plan showing visibility splays and associated landscaping on the spine road should be submitted and approved.

Informatives;

- SP Energy informatives
- Public Rights of Way
- Environment Protection Informatives
- Highways: the applicant will be required to enter into a s38 Agreement regarding the construction and future adoption of the internal road layout.
- Highways: the shared pedestrian/cycle path that runs alongside Flowers Ln should form part of the adopted highway.

In the event of any changes being needed to the wording of the Committee's decision (such as to delete, vary or add conditions/informatives/planning obligations or reasons for approval/refusal) prior to the decision being issued, the Head of Planning has delegated authority to do so in consultation with the Chairman of the Strategic Planning Board, provided that the changes do not exceed the substantive nature of the Committee's decision.



22/2500N

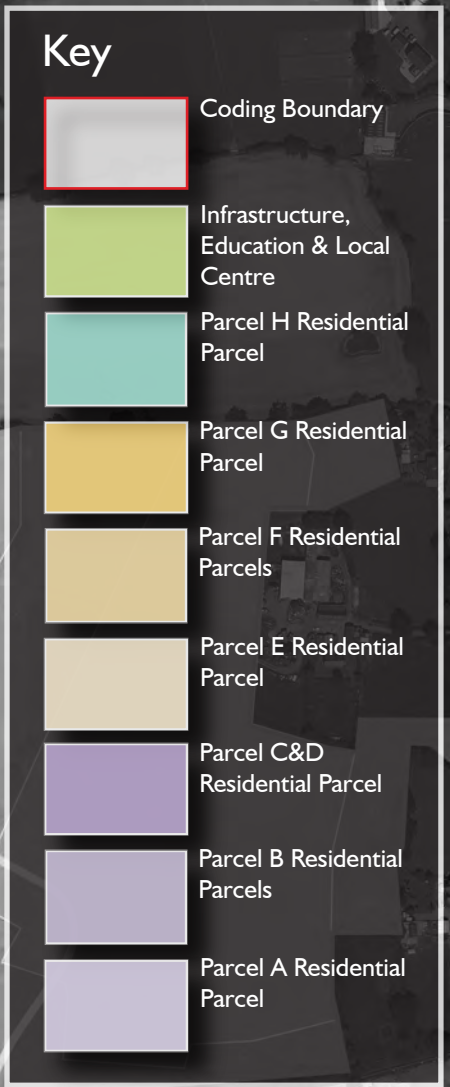
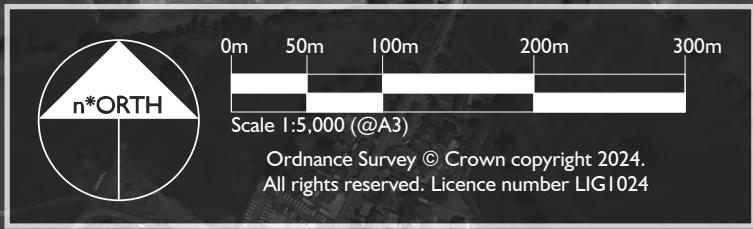
Land Off Minshull New Road
and Flowers Lane

LEIGHTON

Revision.		
A	Boundary updated to match latest planning layout	01.11.23 AF

BLOOR HOMES®

Location Plan - Parcel H



e*SCAPE 15 years
urbanists 2009 - 2024

Project Title
Leighton West, Crewe

e*SCAPE Job No.
021-025

Client
Bloor/Countryside

Drawing Number
021-025-P007

Drawing Title
Parcel & Phase Boundaries

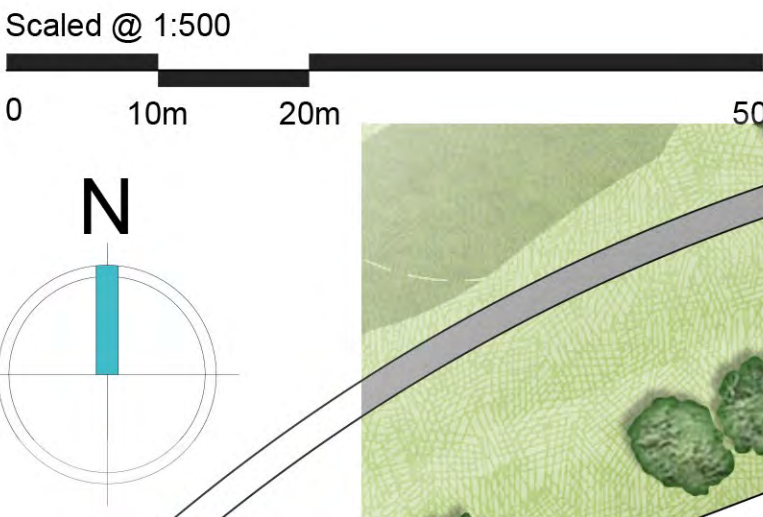
Scale
1:5,000 @ A3

Revision
REV A

Date
May'24

PLANNING LAYOUT KEY

- 1.8m Brick wall
- 2.5m Brick wall entrance feature
- 1.8m Hit & miss timber fence for carport (Parcel F only)
- 1.8m Close boarded timber fence
- Front boundary hedging (see landscapers spec)
- 0.45m Knee rail
- Timber gate
- Bin Storage area
- Bin collection point (bin collection day only)
- Affordable
- 600 x 600 Paving slabs (refer to surface materials plan for clarification)
- Block paving, Stretcher form (refer to surface materials plan for clarification)
- Block pavers (refer to surface materials plan for clarification)
- 1.8m x 0.9m shed position
- Tree/vegetation to be retained.
- Root protection Zone.
- Entrance pillars



Notes:

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All Dimensions are to be verified on site before any work commences. If any discrepancies, errors or omissions are noted, these are to be reported to STEN Architecture immediately.

If any other drawings are referenced within this layout, please refer to the specific detailed drawing for design, materials and specific working practices.



U	Surfacing to squares amended. Railings removed and replace hedges.	TS	13.06.24
T	Plots 31-43 and adjoining highway replanned to remove development further away from RPA.	LS	17.05.24
S	Cycle store removed to plot 13	LS	26.04.24
R	Parking to plot 62 amended.	LS	24.04.24
REV:	DESCRIPTION:	BY:	DATE:

STEN

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The Studio
Harrison Street
Wakefield
WF1 1PS
Tel: 01924 950985

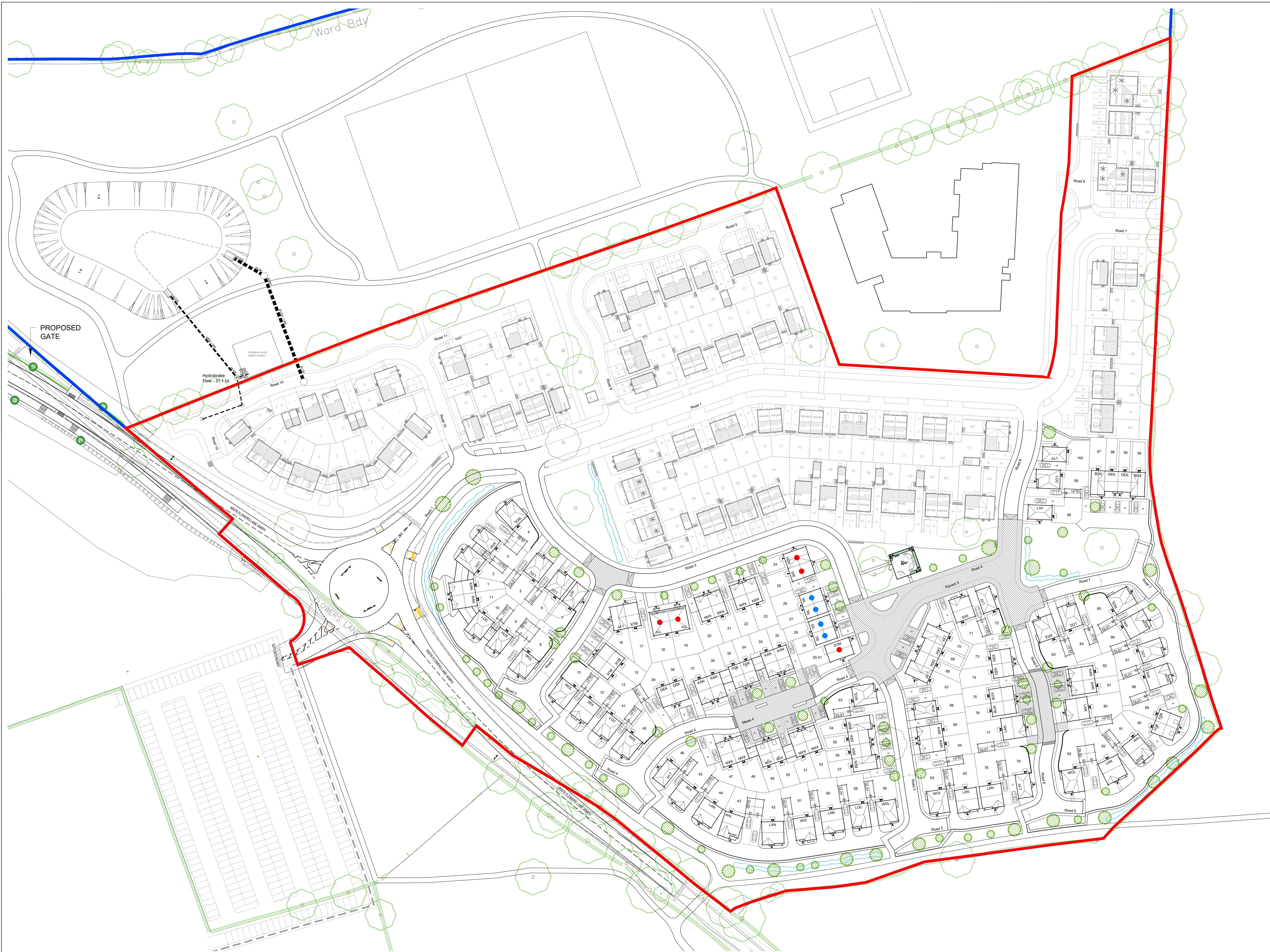
Web: www.sten-architecture.co.uk

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Facebook: [stenarchitectureltd](https://www.facebook.com/stenarchitectureltd)

LinkedIn: [Sten Architecture](https://www.linkedin.com/company/sten-architecture)

CLIENT:	Countryside Partnerships		
SITE:	Leighton West Crewe		
TITLE:	Proposed site layout Parcel H		
SCALE AT A1:	DATE:	DRAWN:	CHECKED:
1:500	29.11.21	TS	SL
PROJECT NO:	DRAWING NO:	REVISION:	
2150	2150.PH.01	U	



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THIS DRAWING IS TO BE READ IN CONJUNCTION WITH SEPARATE GROUP 1 SITE SPECIFIC CONSTRUCTION NOTES AND MATERIALS SPECIFICATION.

Private				90.0%
Type	Beds	Stores	No	
Dekker	2	2	4	
Dekker MT	2	2	6	
Buxton	3	2	9	
Lyford	3	2	6	
Lyford SD	3	2	3	
Kane	3	2	18	
Huxley	3	2	3	
Wixham	3	2	4	
Forbes	3	2.5	6	
Locke	4	2	9	
Wollaton	4	2	10	
Langley	4	2	8	
Wallingford	4	2	4	
			90	

Affordable Rent				6.0%
Type	Beds	Stores	No	
NSS18F03 Symons GF	1	1	1	
NSS18F01 Symons GF	1	1	1	
18B2P Tiverton-M4(3)	1	1	2	
NSS-384P Sisson	3	2	1	
NSS-385P C Sohli	3	2	1	
			6	

Shared				4.0%
Type	Beds	Stores	No	
NSS-283P Silkin	2	2	4	
			4	

Total		100

Denotes dwellings connected with a carport

Denotes Existing Trees (RPA) / Vegetation

Denotes existing trees / vegetation to be removed / thinned out

Proposed estate trees (indicative only, please refer to site landscaping)

Proposed Jacobs trees / vegetation

Denotes footpath (Solid line)

Denotes grassed service verge (Dashed line)

Denotes Sub-Station

Affordable Rent

Shared Ownership

N.B. Landscaping is indicative.
Please refer to detailed landscape drawing.

Revision:

Revision	Date	By
G	18.06.24	AF
F	15.06.24	AF
E	22.05.24	AF
D	30.09.23	AF
C	24.07.23	TJW
B	25.05.23	AF
A	15.03.23	AF

CREWE
Leighton West

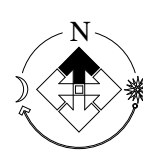
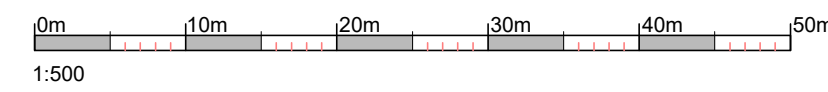
**Site Layout
Phase H**

DATE: 14.03.2022
SCALE: 1:500 @ A0
DRAWN: AF
CHECKED: RTD

BLOOR HOMES

Drawing No. NW112-PD-3001 G

Leighton West, Crewe - Parcel H





Parcel A, Plots 373 - 378



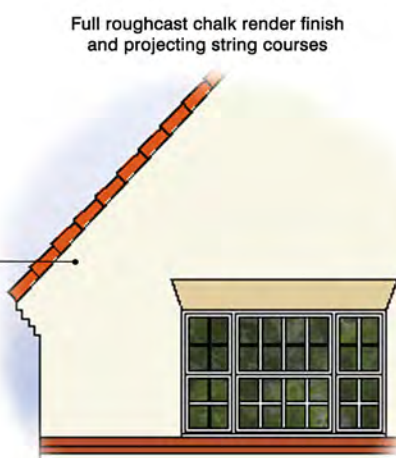
Mock sash style windows with angled stone head and two string courses



Feature arched entrance portico.



Flat portico with wood grain and rolled lead effect top. Individual formal 6 panel styled front door with ironmongery letterbox.



Full roughcast chalk render finish and projecting string courses



Parcel B, Plots 216 - 229



Feature faux chimney in red brick



Square bay with lead rolled top roof



Black tudor board with fixing pegs on chalk render



Square bay with tiled roof projecting over covered porch



Flat portico with wood grain and rolled lead effect top. Contrasting two course string



Square bay with lead rolled top roof



Parcel H, Plots 46 - 58



Parcel A, Plots 373 - 378



Parcel B, Plots 216 - 229



Parcel H, Plots 46 - 58

*Site layouts not to scale

Proposed residential development at Leighton West, Crewe

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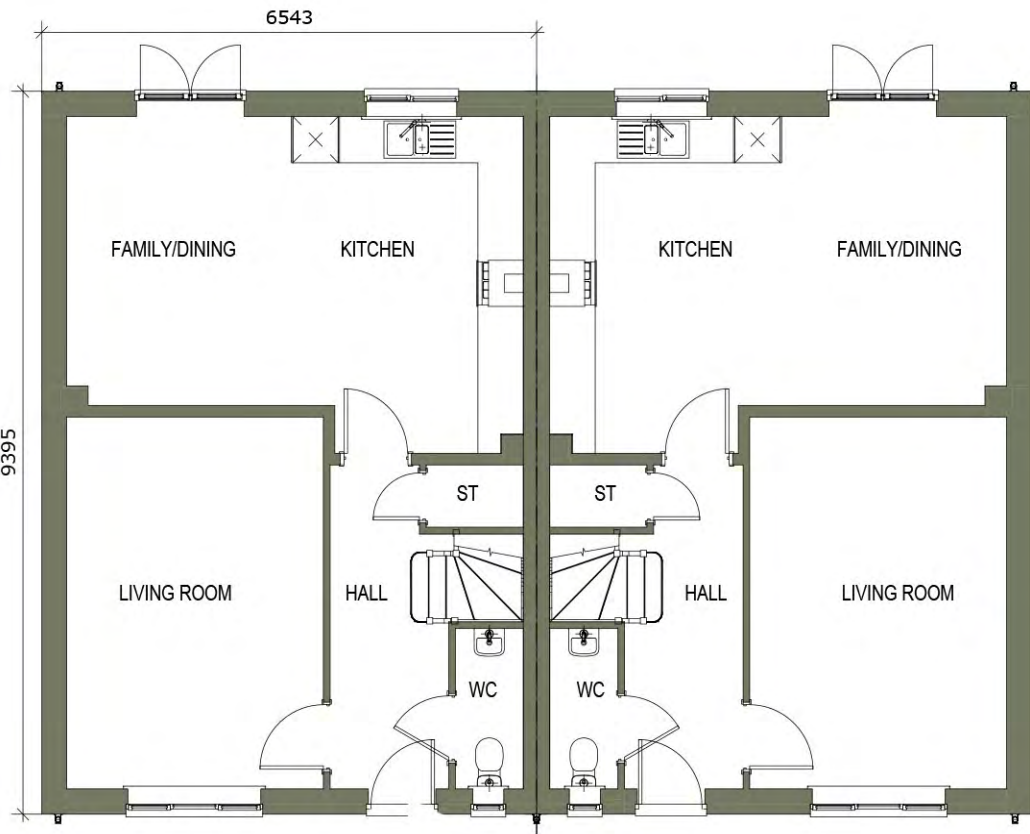
FRONT ELEVATION



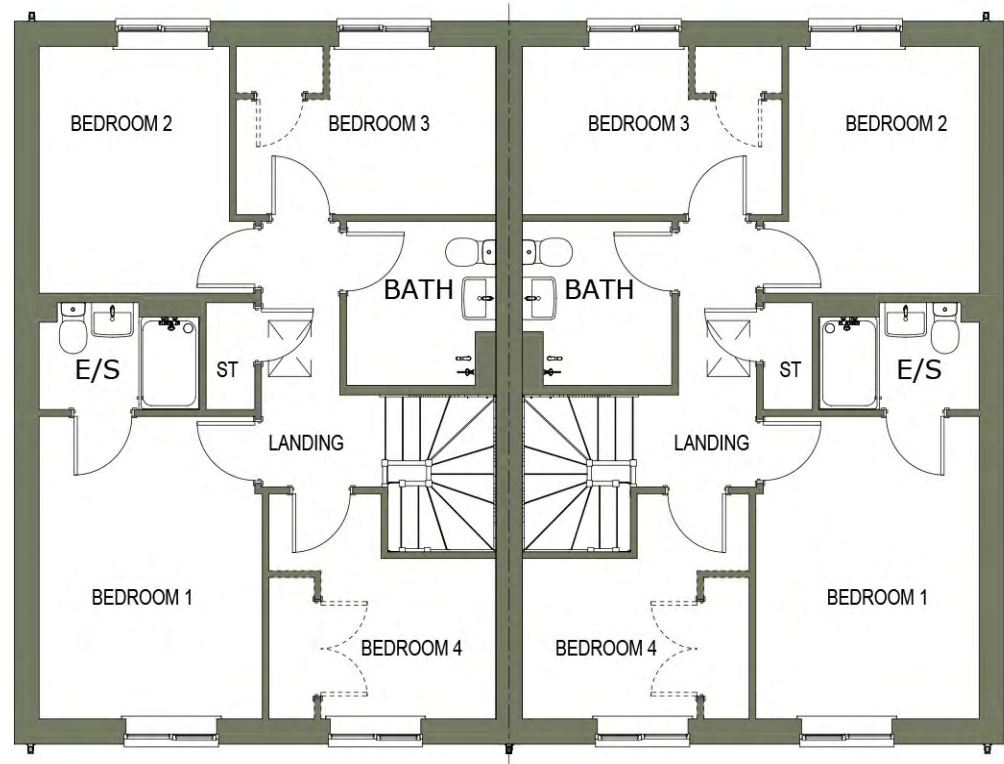
REAR ELEVATION



SIDE ELEVATION



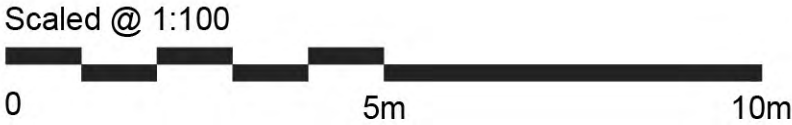
GROUND FLOOR PLAN



FIRST FLOOR PLAN



SIDE ELEVATION



C	Elevations amended to client / council comments.	DS	24.10.23
B	Elevations amended to client / council comments.	DS	24.10.23
A	Elevations amended to client / council comments.	DS	23.08.23
REV.	DESCRIPTION:	BY:	DATE:

STEN
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CLIENT: **Countryside Partnerships**

SITE: **Leighton West Crewe**

TITLE: **Parcel H - Planning Drawings
Myne Pair - Enhanced Elevations**

SCALE AT A3: 1:100	DATE: 05.05.23	DRAWN: DS	CHECKED: SL
PROJECT NO: 2330	DRAWING NO: 2330.MYL.PH.02		
			REVISION: C



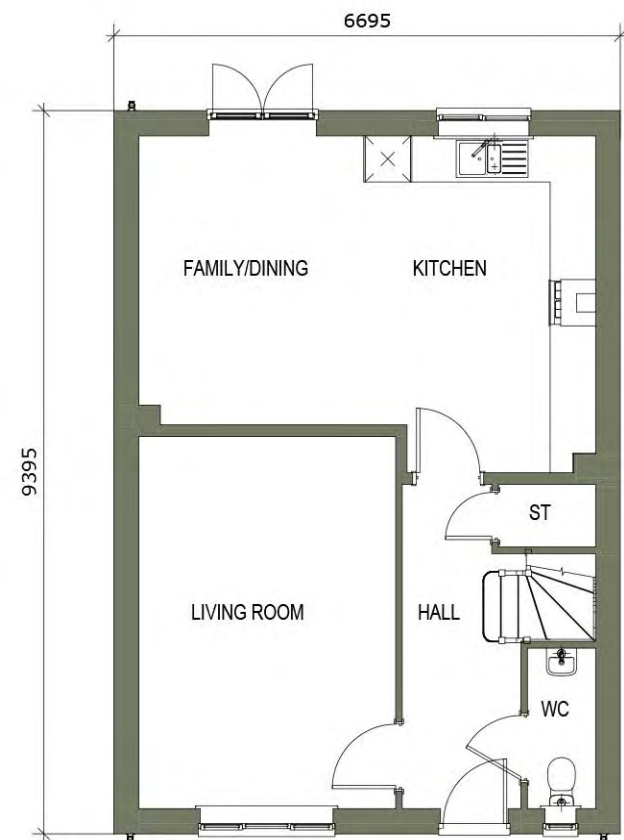
FRONT ELEVATION



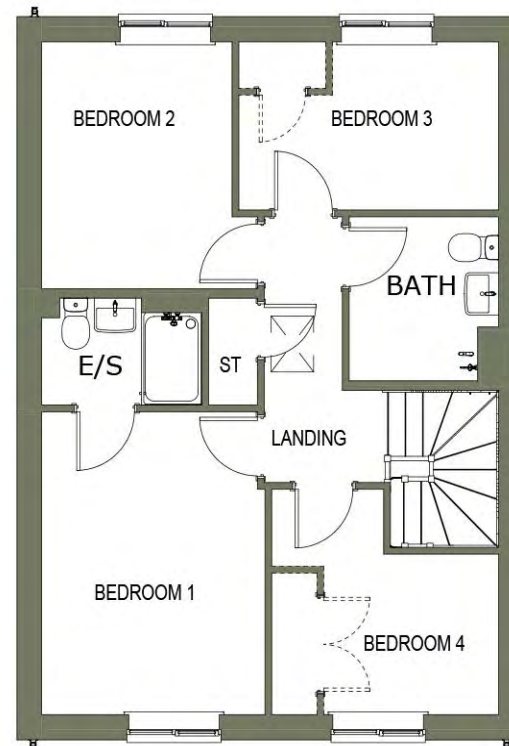
REAR ELEVATION



SIDE ELEVATION



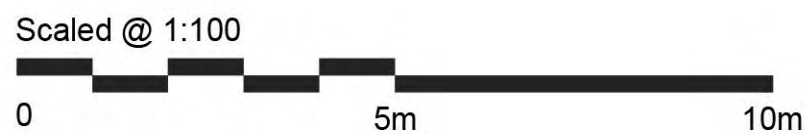
GROUND FLOOR PLAN



FIRST FLOOR PLAN



SIDE ELEVATION



B	Elevations amended to client / council comments.	DS	24.10.23
A	Elevations amended to client / council comments.	DS	23.08.23
REV:	DESCRIPTION:	BY:	DATE:

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CLIENT:	Countryside Partnerships			TITLE:	Parcel H - Planning Drawings Mylne - Standard Elevations		
SITE:	Leighton West Crewe			SCALE AT A3:	DATE:	DRAWN:	CHECKED:
				1:100	05.05.23	DS	SL
				PROJECT NO:	DRAWING NO:	REVISION:	
				2330	2330.MYL.PH.01	B	

Application No: 22/3228N

Location: Land Off Minshull New Road And, FLOWERS LANE, LEIGHTON

Proposal: Reserved matters application for appearance, landscaping, layout and scale with respect to green infrastructure and open space across the outline application area - approved outline application 19/2178N.

Applicant: Bloor Homes and Vistry Partnerships Ltd

Expiry Date: 15-Dec-2022

SUMMARY

This application proposes the Reserved Matters for two areas of land forming the green infrastructure elements of the site, approved in outline and subject to a Section 106 legal agreement, under application reference 19/2178N. It is one of four separate applications being considered on this agenda.

These two areas of land consist of the central spine of the site linking all the sites south of Flowers Lane and the northern most of the sites adjacent to the proposed school site. Several amendments have been made since the application was first submitted, particularly to the footpath network.

Highways have raised no objections, subject to a point of detail being agreed by condition and an informative. The PROW Team have no objections and the proposed footpath/cycleway provision is considered to be good.

The majority of the Ecology and Landscaping issues identified in this report have now been resolved, and although there are some outstanding matters to do with tree impacts, it is anticipated most of these can be addressed and Members will be updated on this matter.

The Council's Flood Risk (LLFA) team have similarly been involved in detailed discussions with the applicant and detailed replies to matters raised have been submitted by the applicant. The LLFA's latest comments were awaited at the time of writing this report, but the main issues have now appear to have been addressed, and if necessary outstanding matters can be conditioned.

There are no objections on the grounds of the proposed areas of public open space, or environmental matters.

RECOMMENDATION

Approve with conditions

SITE DESCRIPTION

This application relates to 2 separate areas of land that sit outside the development areas described in the sites also on this agenda.

The first site lies along the northern boundary of the site adjacent to open countryside on the edge of Crewe. The second site – which is a series of separate sites at present, runs southwest to northeast, and follows the route of the overhead power lines. All the land is currently in agricultural use.

There are hedgerows with some trees to the site's field boundaries, and some scattered trees and remnants of hedgerows within the sites themselves, however they are limited in number and the sites are generally open in character.

A farm complex lies to the west fronting Middlewich Road, and Leighton Hospital is in close proximity, although does not adjoin the site(s).

There are no conservation areas on or adjoining the site, and there are no listed buildings on or adjacent to the site.

A public footpath, Leighton FP3 runs east west across the site, with a spur which runs south as Leighton FP6 which becomes Crewe FP20.

The sites fall in Flood Zone 1 – Least risk of flooding.

PROPOSAL

The application title reads:

“Reserved matters application for appearance, landscaping, layout and scale with respect to green infrastructure and open space across the outline application area - approved outline application 19/2178N.”

Reserved Matters approval is sought for Appearance, Landscaping, Layout & Scale.

The application is effectively the open space – which is multifunctional, to serve the residential, and to a lesser extent the commercial development, also being considered on this agenda.

Part of the site forms the northern part of LPS5, whereas the rest forms part of allocation LPS6.

The application consists of the following main elements:

- Formal and informal footpaths and cycleways
- Formal play areas
- Informal open space
- SuDs & water attenuation
- Landscaping – existing retained and proposed including gateway features
- Habitat creation
- Allotments and community orchard.

The triggers for the creation of these areas of open space are contained within the signed Section 106 Agreement that accompanied the outline approval.

An Environmental Impact Assessment (EIA) was submitted with the outline application.

RELEVANT PLANNING HISTORY

Outline planning permission was granted under:

19/2178N Outline planning approval for the development of up to 850 residential units (Use Class C3), land reserved for new primary school, a local centre (Use Class A1-A4, AA, B1a, C3 and D1) and associated infrastructure and open space. Land off Minshull New Road and FLOWERS LANE, LEIGHTON APPROVED Nov 2021

In addition, planning approval was granted for a series of Highway works in the immediate vicinity:

18/6118N A proposed series of highway infrastructure measures and associated works, in the Leighton area of Crewe, and known as the North West Crewe Package - Land Between Flowers Lane Minshull New Road The A530 Middlewich Road And North Of, PYMS LANE, CREWE APPROVED March 2019

Finally, this is one of four reserved matters applications submitted for Leighton West. All are on this agenda. The others are:

22/2476N Reserved Matters application for approval of appearance, landscaping, layout and scale following Outline approval 19/2178N for erection of 304 dwellings on Parcels A and B Land Off Minshull New Road And, FLOWERS LANE, LEIGHTON

22/2499N Reserved matters application for appearance, landscaping, layout and scale with respect to 304 dwellings on Parcels C, D, E, F and G. An Environmental Statement was submitted at the time of the outline application. Land Off Minshull New Road And, FLOWERS LANE, LEIGHTON

22/2500N Reserved matters application for appearance, landscaping, layout and scale with respect to 200 dwellings on Parcel H. An Environmental Statement was submitted at the time of the outline application. Land Off Minshull New Road And, FLOWERS LANE, LEIGHTON

Other approvals under LPS 5, immediately to the south east include:

16/2373N Outline application for the construction of up to 400 dwellings with garaging; parking; public open space; landscaping; new vehicle and pedestrian accesses; highway works, foul and surface water drainage infrastructure and all ancillary works. - Land At, Flowers Lane, Leighton APPROVED January 2018

20/3210N Reserved matters approval sought for access, appearance, landscaping, layout and scale. following outline permission for the construction of up to 400 dwellings with garaging; parking; public open space; landscaping; new vehicle and pedestrian accesses; highway works, foul and surface water drainage infrastructure and all ancillary works. Land At, FLOWERS LANE, LEIGHTON APPROVED July 2021

This development is now on site.

Finally, there are numerous approvals at Leighton Hospital, the most relevant recent approval being a car park on the north eastern side fronting Flowers Lane.

POLICIES

Cheshire East Local Plan Strategy (LPS) – 2010-2030

PG1 – Development Strategy
PG6 – Open Countryside
SD1 - Sustainable Development in Cheshire East
SD2 - Sustainable Development Principles
IN1 – Infrastructure
IN2 – Developer Contributions
SC1 – Leisure and recreation
Sc2 – Indoor and outdoor recreation
SE 1 - Design
SE 2 - Efficient Use of Land
SE3 – Biodiversity and Geodiversity
SE 4 - The Landscape
SE 5 - Trees, Hedgerows and Woodland
SE 6 - Green Infrastructure
SE 13 - Flood Risk and Water Management
CO1 – Sustainable Travel and Transportation

LPS4 – Leighton West
LPS5 – Leighton

Site Allocations and Development Policies Document (SADPD) 2022

GEN 1 - Design principles
GEN 4 - Recovery of forward-funded infrastructure costs
ENV 1 - Ecological network
ENV 2 - Ecological implementation
ENV 5 – Landscaping
ENV 6 - Trees, hedgerows and woodland implementation
ENV 7 - Climate change
ENV 14 - Light pollution
ENV 16 - Surface water management and flood risk
HOU 1 - Housing mix
HOU 8 - Space, accessibility and wheelchair housing standards
HOU 13 - Residential standards
HOU 14 - Housing density
HOU 15 - Housing delivery
INF 1 - Cycleways, bridleways and footpaths
INF 3 - Highway safety and access
INF 6 - Protection of existing and proposed infrastructure
INF 9 – Utilities
REC 3 - Open space implementation
REC 5 - Community facilities

Neighbourhood Plans:

Crewe has not made any progress towards making a Neighbourhood Plan, and Minshull Vernon is at Regulation 7 Stage: Designated Neighbourhood Area, but at this stage can be given very little weight.

Other Material Considerations

The National Planning Policy Framework

CONSULTATIONS (External to Planning)

SP Energy Networks: No objections are raised, but they remind the applicant of their health & safety responsibilities around the significant electricity infrastructure which passes through the site, and the safe working practices that should be employed.

CEC Head of Strategic Infrastructure: No objections, subject to conditions and an informative.

CEC Public Rights of Way: No objections

CEC Environmental Health: No objections are made, subject to reminding the applicant of their responsibilities to discharge the conditions under the outline approval relating to amenity and air quality, and the contaminated land team recommends a further condition. Informatives are recommended.

CEC Flood Risk Manager: Extensive discussions have taken place and updated comments are awaited at the time of writing the report.

ANSA: Now that the range of facilities has been clarified across the site they raise no objections, however they recommend the detailed design of facilities is conditioned.

VIEWS OF THE TOWN/PARISH COUNCILS

Minshull Vernon and District Parish Council: No comments received

Crewe Town Council: They write:

“The committee objects to the principle of development on the site without investment in health and education infrastructure. However, the play and leisure facilities proposed will be a positive benefit. It is unclear how these will be maintained and who will be responsible for shared space maintenance and management. This should be detailed within any planning application and associated approval/decision.

It is advised and sought by Crewe Town Council’s Planning Committee that the play and leisure facilities include for the provision of CCTV to be included within the Cheshire East centralised CCTV monitoring unit.”

OTHER REPRESENTATIONS

One representation has been received from a local interest group who write:

“The scheme appears to be well thought out and attractive and St Peter's Church, Leighton-cum-Minshull Vernon would welcome its solid proposals. Our interest lies mainly in some financial assistance from the

S106 to support the potential increase in parishioners future spiritual needs that our small church will have to provide once the development is complete.”

OFFICER APPRAISAL

Principal of Development

The site is allocated in Local Plan policies LPS 4 and LPS 5 and has the benefit of outline planning permission as set out above. The outline set out the requirements for the site, and the Section 106 contributions/requirements. The policies read:

LPS 4 Leighton West, Crewe

Development at Leighton West will result in the creation of a new sustainable neighbourhood to support Leighton Hospital and provide a key site for the development of an automotive research, development and supply hub, in partnership with Bentley Motors which is located in very close proximity to the site. The development of Leighton West over the Local Plan Strategy period will be achieved through:

1. Contributions to health infrastructure and the provision of land adjacent to Leighton Hospital, for its future expansion, to ensure that the future health care needs of the area can be met;
2. The delivery of around 850 new homes (at a variety of densities). The design, density and scale of the development should reflect the fact that the site lies in a transitional location between the higher density urban area and the rural area. The surrounding development is predominantly suburban and the development of the site should reflect this. The development of the site will be masterplan-led, including a design code, which will consider its location, constraints and opportunities;
3. The inclusion of key worker housing, for the employees of Leighton Hospital;
4. The delivery of a new mixed-use local centre that will serve Leighton Hospital and nearby residents including:
 - i. Provision of retail appropriate to meet local needs;
 - ii. Community facilities;
 - iii. Public House;
 - iv. Children's day nursery;
 - v. A new primary school; and
 - vi. Leisure facilities;
5. Around 5 ha of additional employment land located at the southern end of the site including a science/energy park which could include advanced/automotive engineering and manufacturing;
6. The incorporation of green infrastructure, including:
 - i. Green corridor;
 - ii. Allotments;
 - iii. Open space including formal sports pitches, multi-use games area, outdoor gym, equipped children's play space and facilities for teenagers;
7. A new bus interchange for the hospital and nearby residential areas;
8. On land to the north of Pym's Lane, there is potential for a science/energy park and delivery of a key site for the development of an automotive research, development and supply hub, working in partnership with Bentley Motors. This area also has the potential to include a geothermal plant and district heating hub;
9. The widening and/or realignment of Smithy Lane, to provide access to the site and improved access to Leighton Hospital for emergency vehicles and suitable footpath and cycle lanes;

10. A package of network improvements to provide improved access to Leighton Hospital and surrounding residential development including emergency vehicles and suitable footpath and cycle lanes; and
11. An improved 'emergency portal' for blue light vehicles being able to access the hospital's Accident and Emergency Department from Smithy Lane.

Site LPS 5 Leighton, Crewe

This site lies adjacent to the Strategic Site of Leighton West. The site lies in close proximity to both Leighton Hospital and Bentley Motors; its development will result in the expansion of the new sustainable neighbourhood at Leighton West. This will provide opportunities for people to live near to the key automotive hub, to be located at the southern end of the Leighton West site and in close proximity to Bentley Motors. The development of the Leighton West Strategic Site over the Local Plan Strategy period will be achieved through:

1. The delivery of around 500 homes (at a variety of densities). The design, density and scale of the development should reflect the fact that the site lies in a transitional location between the higher density urban area and the rural area. The surrounding development is predominantly suburban and the development of the site should reflect this. The development of the site will be masterplan-led, including a design code, which will consider its location, constraints and opportunities;
2. Further road improvements to upgrade access to Leighton Hospital for emergency vehicles and suitable footpath and cycle lanes;
3. Key worker housing to be provided, for the employees of Leighton Hospital;
4. Site to be designed to complement the allocated site at Leighton West; and
5. Incorporation of green infrastructure, including:
 - i. A linear green corridor through the site, including the land below and adjacent to the pylons;
 - ii. Allotments;
 - iii. Open space including formal sports pitches, multi-use games area; outdoor gym and equipped children's play space.

This site, which proposes the green infrastructure with its various facilities, takes up the northern part of LPS5 – the southern half being subject to an application for housing in Parcel H, together with another approved housing scheme now on site. In addition, it takes up the central area of LPS4 with the remaining areas being subject to reserved matters housing applications also on this agenda, or in the case of the local centre or hospital expansion being subject to future proposals.

Clearly this application only makes up a part of these sites, but nevertheless an important part, in principle satisfying the green infrastructure elements of the policies.

Highway Implications

This site forms part of the local plan allocation LPS4 and already has outline approval where access was determined including the wider highways impact and any mitigation required. This current proposal is the reserved matters for the section of land consisting mainly of landscaping area that runs through the middle of the site and playing fields along the northern area.

There will be pedestrian connections through this site connecting the different parcels of development. There will be no carriageway through the site but there will be a cycle route.

A turning head at the end of Flowers Ln, where it will be closed off to vehicles, is required and this can also be conditioned. The shared pedestrian/cycle path that runs alongside Flowers Ln should be adopted and built to adoptable standards.

Subject to these conditions and an informative, no objection is raised.

Public Rights of Way/Cycle routes:

The PROW Team write that the footpaths, Leighton 3 & 6 appear to be accommodated within the landscape plans although it would have been helpful to have the definitive routes shown on the plans.

As discussed in the other applications, the applicants have submitted a Landscape Masterplan which combines all the sites and show the distribution of (amongst other things) the footpaths/cycleways. Following input from the Council's Landscape Architect these routes have been increased to make better linkages between the parcels and to provide circular routes for exercise etc.

More strategic footpath and cycle routes are created to re-enforce those provided as part of the North West Crewe Package highway works to allow users to travel away from highway frontages and give other route options, which is welcomed.

The overall provision is considered to acceptable and should encourage residents to walk and cycle, in line with policy objectives.

Landscape

The Council's Landscape Architect suggested the following changes to the green infrastructure areas in the original landscape plans for these areas:

- Introduce more/greater circular routes within the whole linear park:
- Introduce smaller ones around the SUDs basins
- With the larger circular routes which follow at a slight distance the housing estate edges introduce an element of safety/security e.g. for lone walker/runners, a long route.
- Straighten the central route where possible to create a strong designed route which follows the pylon routes.
- Create a few hard landscaped spaces along the central route which act as destinations, resting points, points of interest e.g.: seating area with an artwork overlooking an orchard etc. Emphasise crossing footpaths.
- Future proof footpath network especially along the pylon route by creating subtle 'ghost' connections to potential future park extensions, land release, etc.
- Increase perpendicular cross park connectivity between housing and other areas, maybe using more curved footpath routes.
- Strengthen the relationship between park and the 'local' centre
- Iron out unnecessary kinks in paths where possible
- The scheme could benefit from some strong structured low/medium sized tree planting, i.e. an avenue of apple trees, more hedgerows to some define routes, obviously larger trees should be avoided under the pylons.

The recently submitted Landscape Masterplan addresses the majority of these concerns, introducing more informal footpaths and improving linkages, tother with creating focal points. The individual

landscaping plans need to be updated to reflect these changes, but it is considered this can be conditioned now that the basic principles have been established.

Trees

An amended Arboricultural Impact Assessment (AIA) and an updated Arboricultural Method Statement (AMS) has been submitted further to the provision of the earlier forestry comments. Clarification has been provided regards several of the issues raised as requiring more detail and explanation.

It is understood that tree and hedgerow losses which have occurred since the preliminary survey was carried out at Outline have arisen because of Highway Infrastructure projects and the supporting plans are now annotated to confirm where this is believed to be the case. The TPO trees (Group G7) of the survey are shown for removal as this route into the site is reported to be an agreed bus link into the site from the neighbouring development area and already agreed.

The updated AIA and AMS has now been broken down by phase of development and makes provision to show all trees including those presently shown for removal shown by tree quality category colour with RPAs, so that impacts can be better understood. Some explanatory notes have been provided regards the pre agreed loss of TPO'd trees and those trees already removed for the highway's infrastructure projects.

The AIA indicates that of the trees surveyed across the whole site that 4 individual and 1 group of high quality A Category tree, 15 individual and 6 moderate quality B Category trees, 22 individual and 2 groups of low-quality C Category trees and 1 individual and 1 group of poor quality U Category tree as well as a combined total of 711 linear metres from 9 hedgerows which are proposed for removal to accommodate the proposals associated with the whole development area. An updated AMS (Version 3) has also been provided.

The submitted Assessment of Important Hedgerows considers the whole of the site and has not been broken down to impacts on the submitted applications. Twelve hedgerows (H1-H12) have been identified as important in accordance with criteria 5a of the Hedgerow Regulations.

5. The hedgerow:

a) is recorded in a document held at a Record Office as an integral part of a field system pre-dating the Inclosure Acts

Hedgerow 1 is also importance in accordance criteria 1.

1. Any hedgerow that marks the boundary or part of the boundary, of at least one historic parish or township; and for this purpose, "historic" means existing before 1850.

The assessment confirms that sections of hedgerow H2, H5, H6, H7 AND H8B0 are to be removed as part of proposed development for Northwest Crewe Package and includes the removal of sections for Highway works. Additional removal of H3 is required for a bus route and access. The overall development will arise in the partial loss of hedgerows which form a 19th Century field system but most of the hedgerows are shown for retention where possible and the report has concluded that the overall harm is; 'less than substantial'. Mitigation is proposed of 2,300m of new hedgerows. As hedgerows have been found to be 'Important' under the above referenced criteria of the Regulations, the loss of the hedgerows to accommodate the development is a material consideration in the determination of the application although it appears that sufficient compensatory planting is being offered across the whole development area.

Having appraised the amended detail and proposals identified it is unclear why so many high quality and moderate quality trees are being removed within some areas and these have been referenced more specifically in association with applications relating to the parcels for development.

Initial observations suggest that the landscape proposals as indicated for the Flowers Lane Transition POS which is to the north of the Parcel H has not been considered in any detail. Trees around and within the perimeter of the area are shown in the supporting AIA and AMS, but provision has not been made to afford any protection to the trees during any approved implementation period. Elements of the landscape proposals, such as the detention basins south of trees along the northern boundary are annotated as 'to be confirmed' in terms of final design and size which presents concerns regards unquantified impacts to trees.

It is noted that discrepancies exist between the CGI (Combined Green Infrastructure) Planting Plans and the tree retention and removals plans associated with the AMS with trees shown to be retained on the CGI plans which are down for removal in the arboricultural report for example; CGI Planting Plan – Sheet 8 (G101-08 Rev P12) shows trees T27, T37 and group G4 as being retained whereas the AMS suggests they will be removed and this seems to be repeated on several of the 29 planting plans.

The AMS as submitted does include a tree protection plan, make provision for a methodology for working in relation to retained trees, and identifies areas where special measures would be implemented in respect of minor root pruning and construction of engineer designed surfaces although as identified in some areas tree protection is missing in relation to the site edged red of this application and potential impacts are yet to be confirmed. Final comments will be provided once the pond detail to the north of Parcel H has been submitted. It is recommended that the final design and size of the ponds should ensure that no further losses will arise to any moderate quality trees within the area. Tree Protection, levels information and a working methodology to minimise the effects of any works in the area should also be provided.

For completeness in the assessment and determination of this planning application, as hedge loss is involved it is considered the hedgerows should be assessed against all criteria in the Hedgerow Regulations 1997 in order to ascertain if they qualify as 'Important'. The Regulations require assessment on various criteria including ecological and historic value. Should any hedgerow shown for removal be found to be 'Important' under any of the criteria in the Regulations, this would be a material consideration in the determination of the application. Hedgerows are also a habitat subject of a Biodiversity Action Plan.

The applicant has been looking at the impacts on trees identified above as part of the revised plans, as whilst the Council's Tree Officer has not formally objected to the application on the basis of tree impacts, at least some of these impacts would seem to be avoidable. At the time of writing this report the applicant was discussing amendments with the Council's Tree Officer which would allow for the retention/better protection of more trees, and Members will be updated on this matter in an Update Report.

Ecology

A number of conditions were attached to the outline consent at this site.

Condition 9 Ecological Monitoring Strategy:

Monitoring proposals are required by this condition are included in the submitted Habitat Management Plan TEP Version 3.0 dated 10.4.24.

Condition 11 Detailed design of attenuation ponds:

A drainage statement has been submitted (Lees Roxborough Rev A). No detailed designs of attenuation ponds have been submitted with this reserved matters application as required by this condition. The Council's Ecologist requests these be submitted. It is advised that the attenuation ponds must be designed to hold an area of permanent open water.

Condition 12 Updated hedgerow impact assessment, mitigation and compensation strategy:

The submitted biodiversity assessment (January 2024) however advises that 2.6km of hedgerow would be lost across the entire outline site under the various reserved matters applications, with 2.17km of new hedgerow planting being provided. The originally submitted BNG metric concluded that the reserved matters applications would result in a net loss of 10.41% in respect of hedgerow biodiversity.

It was advised that the development proposals must ensure that the extent of hedgerow retained as part of the development is maximised and that any unavoidable losses of hedgerow are adequately compensated for by appropriate native species hedgerow planting. A greater length of replacement planting to that lost will be required to fully address the loss of biodiversity from hedgerow removal. Ornamental hedgerow planning is not acceptable as compensation for the loss of native species hedgerow.

Following the submission of further information to clarify the impacts of this development as opposed to losses for the road scheme – which appears to have caused the confusion, the Council's Ecologist comments:

"The proposed reserved matters application combined will result in the loss of 0.38km of hedgerow, including lengths of hedgerow considered to be 'Important' under the Hedgerow Regulation. The BNG metric has been undertaken on the basis of 2.17km of new hedgerow planting being provided on site. This is based on PGLA Landscape Masterplan Rev 11.

The proposed hedgerow planting would result in a net gain of hedgerows of 30.57%. Therefore, if the loss of the existing hedgerows is considered unavoidable sufficient planting would be provided to compensate for that lost. This would however be dependent upon the reserved matters application being brought forward in accordance with the landscape master plan."

This masterplan would, should planning permission be granted, form one of the approved plans.

Condition 25 Updated badger and barn owl survey and mitigation strategy:

Acceptable updated barn owl and badger survey and mitigation strategy has been submitted as required by this condition.

Condition 26 Details of any proposed lighting. The lighting strategy should reflect the Bat Conservation Trust Guidance Note:

The covering letter from the Agent refers to a lighting strategy being included in the submitted 'Ecological Monitoring Strategy' however an 'Ecological Monitoring Strategy' does not appear to have been submitted in support of this application. The applicant proposes that a detailed design for the lighting (in accordance with the strategy) be submitted under condition 22 prior to installation of the lighting.

Condition 27 25 year habitat management plan:

An acceptable Management plan has been submitted (Habitat Management Plan TEP Version 3.0 dated 10.4.24).

Condition 28 The first reserved matters application shall be supported by a strategy for the incorporation of features to enhance the biodiversity value of the proposed development:

A Biodiversity Impact Assessment (TEP June 2022) which covers the full extent of the outline planning consent subject to the various current reserved matters applications was previously submitted in support of this reserved matters application. A revised BNG metric dated 22/1/24 has also now been submitted. The metric includes measurements for the areas of various habitats to be provided on site. The area of species rich grassland provide don site is less than that required by condition 28 as is the extent of hedgerow planting. The area of cornfield annuals and wet grassland is however greater than required by condition 28.

The assessment concludes that the proposed development (based on all of the reserved matters applications) would deliver a net gain for area based habitats of 12%, but a net loss of -10.41% of hedgerow biodiversity.

Proposals for the provision of bird and bat boxes and gaps in garden fences have been submitted as required by this condition. It must however be ensured that bat/bird boxes are proposed to be attached to 30% of consented dwellings in accordance with the Cheshire East Design Guide.

Detailed method statements for the creation of species rich grassland and cornfield annual meadow habitats are outstanding. These must be submitted prior to the discharge of this condition.

Bat survey

Two additional trees have been identified as requiring removal under this series of reserved matters application (T82 and T122). This application is supported by a letter from the applicant's ecological consultant advising that a bat survey/assessment of these trees is required. A further inspection of these trees has been undertaken and the trees have been found to be unsuitable for roosting bats. No further action in respect of roosting bats is therefore required.

Additional conditions

If reserved matters consent is granted a condition would be required to safeguard nesting birds.

Whilst ideally all matters would have been resolved now, outstanding matters relating to the discharge of conditions highlighted above, will be picked up in their subsequent discharge.

Urban Design

As this application only relates to the areas of green infrastructure the Council's Urban Design Officer has not commented on this application specifically – leaving it to the Council's Landscape Architect. Comments have been made on individual applications where there is an interface with development parcels.

Environmental Impacts

Environmental Protection have confirmed they have no objections to the application with regards to general amenity (including noise) and air quality subject to satisfactory discharge of the relevant conditions on the outline and informatives. The contaminated land team comment that they have no objection to the application subject to the following comments with regard to contaminated land:

- RoC Consulting Phase 1 Desktop Study was submitted in support of the outline application.
- The report identifies a number of potential pollutant linkages that require further investigation. The report recommends that a Phase II investigation is conducted at the site.
- A rigorous monitoring-based ground gas risk assessment will be required to assess the potential impact of ground-gas migration from the landfills adjacent south.

As such, and in accordance with the NPPF, the Contaminated Team recommends that that conditions, reasons and notes be attached should planning permission be granted.

Flood Risk/Drainage

Extensive discussions have taken place with the applicant's drainage engineers, with matters of relating to drainage calculations, finished floor levels, management arrangements and details of a pumping station being discussed. The applicant has submitted detailed replies to all matters raised, but at the time of writing this report the Flood Risk Manager's updated comments are awaited. It is considered that if there are any outstanding matters raised, they could be conditioned. It is important to note that of course this application proposes very little built development – only footpaths etc but is still part of the drainage plan containing numerous SuD's features so needs to be considered alongside the residential parcels.

Public Open space

The public open space provision was set out at the outline stage and captured by the Section 106 Agreement. Overall, the site includes the following facilities:

- Multi Use Games Area (MUGA)
- Teen Shelter & Play
- Neighbourhood Equipped Area of Play (NEAP)
- An orchard
- Allotments
- Local Equipped Areas of Play (LEAP)
- Local Areas of Play (LAP) - 6 No.
- Areas of outdoor gym equipment
- Areas of incidental open space

Many of these facilities are included with the common areas of green infrastructure, subject to this application, which runs through the centre of the site, and along the northern boundary where the MUGA, Teen facilities and NEAP are located. This is considered to be a good approach as they offer good accessibility to many of the facilities, with only a limited number of play areas being sited in the housing areas where it was more appropriate.

Due to the way the information was presented, ANSA where initially unclear what provision was being made, however following the submission of a Landscape Masterplan which sets it out more clearly they have confirmed they have no objections to the provision as submitted. There are however detailed design issues with the individual areas of provision, so ANSA have asked for the detailed design of each of the items listed above (incidental open space excluded) to be subject of a condition.

Other matters

St Peter's Church have raised the issue of a Section 106 contribution towards their church. Whilst this is understood, unfortunately as a Reserved Matters application a Section 106 contribution cannot be requested, as it can only be required at the full or in this case outline stage of the application.

The Town Council raise the issue of management & maintenance of the green infrastructure, which is an important point, but this was addressed at the outline stage by provisions in the Section 106 Agreement to ensure these aspects are agreed in advance.

With regards to CCTV, it is not something that would normally be a requirement of any development of this nature unless there is a specific identified need, which is not considered applicable here.

CONCLUSIONS

This application proposes the Reserved Matters for two areas of land forming the green infrastructure elements of the site, approved in outline and subject to a Section 106 legal agreement, under application reference 19/2178N. It is one of four separate applications being considered on this agenda.

These two areas of land consist of the central spine of the site linking all the sites south of Flowers Lane and the northern most of the sites adjacent to the proposed school site. Several amendments have been made since the application was first submitted, particularly to the footpath network.

Highways have raised no objections, subject to a point of detail being agreed by condition and an informative. The PROW Team have no objections and the proposed footpath/cycleway provision is considered to be good.

The majority of the Ecology and Landscaping issues identified in this report have now been resolved, and although there are some outstanding matters to do with tree impacts, it is anticipated most of these can be addressed and Members will be updated on this matter.

The Council's Flood Risk (LLFA) team have similarly been involved in detailed discussions with the applicant and detailed replies to matters raised have been submitted by the applicant. The LLFA's latest comments were awaited at the time of writing this report, but the main issues have now appear to have been addressed, and if necessary outstanding matters can be conditioned.

There are no objections on the grounds of the proposed areas of public open space, or environmental matters.

RECOMMENDATION

Approve subject to the following conditions;

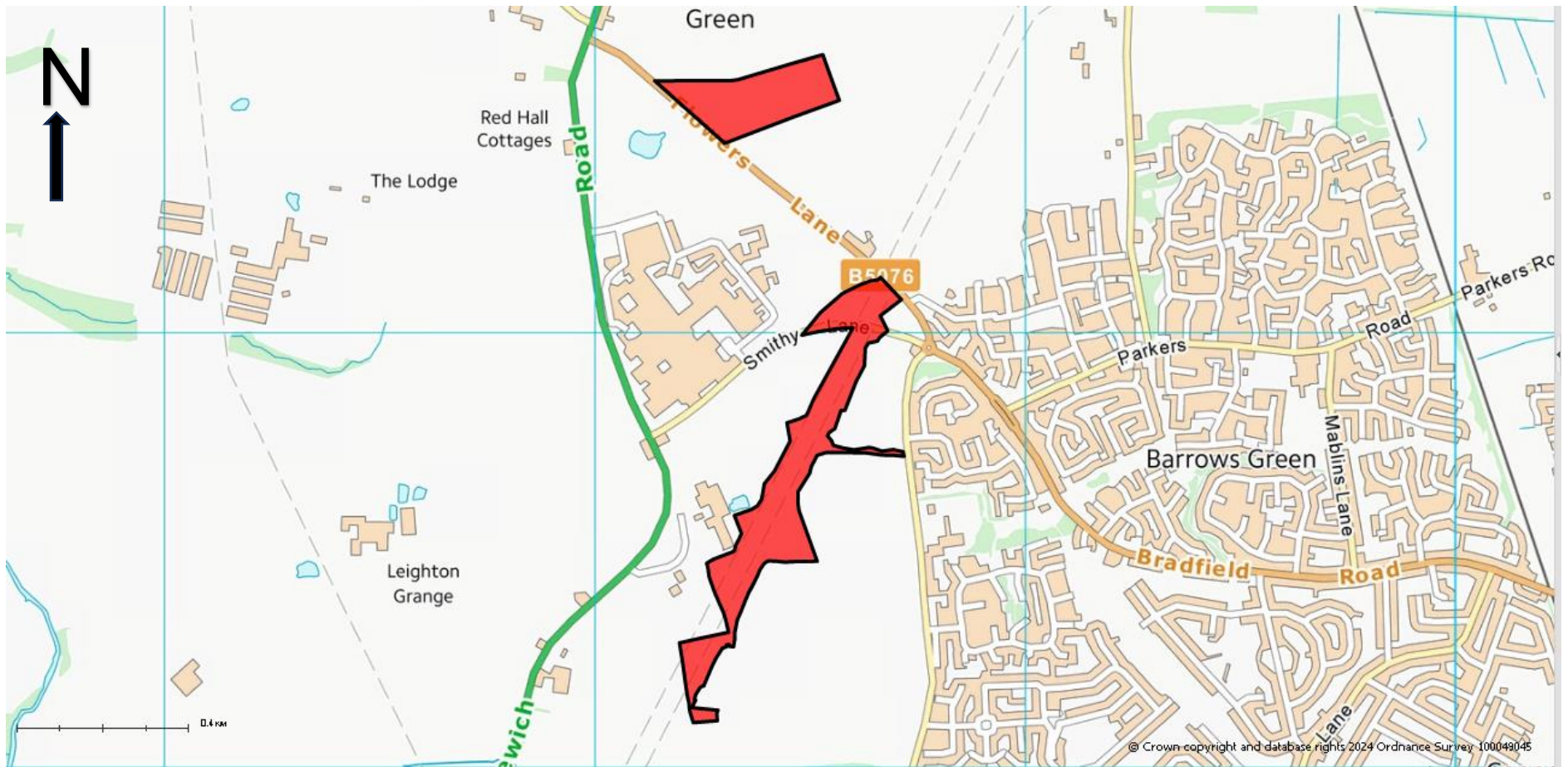
1. Approved plans
2. Bird nesting
3. Submission of a Phase II ground investigation and risk assessment
4. Submission of a Verification Report
5. Soils testing
6. Unexpected contamination

7. Detailed design of the individual elements of play areas/Teen facilities/MUGA/outdoor gym, allotments and orchard to be agreed.
8. Detailed landscaping to be submitted and approved
9. A plan detailing a turning head at the end of the closed section of Flowers Ln should be submitted and approved.

Informatives;

- SP Energy informatives
- Public Rights of Way
- Environment Protection Informatives
- Highways: the shared pedestrian/cycle path that runs alongside Flowers Ln should form part of the adopted highway

In the event of any changes being needed to the wording of the Committee's decision (such as to delete, vary or add conditions/informatives/planning obligations or reasons for approval/refusal) prior to the decision being issued, the Head of Planning has delegated authority to do so in consultation with the Chairman of the Strategic Planning Board, provided that the changes do not exceed the substantive nature of the Committee's decision.



22/3228N

Land Off Minshull New Road
and Flowers Lane

LEIGHTON

Revision:		
A	Updated to match latest planning layout	02.02.24 AF

Vistry
Partnerships

CREWE
Leighton West

**Application Boundary
Common Infrastructure**

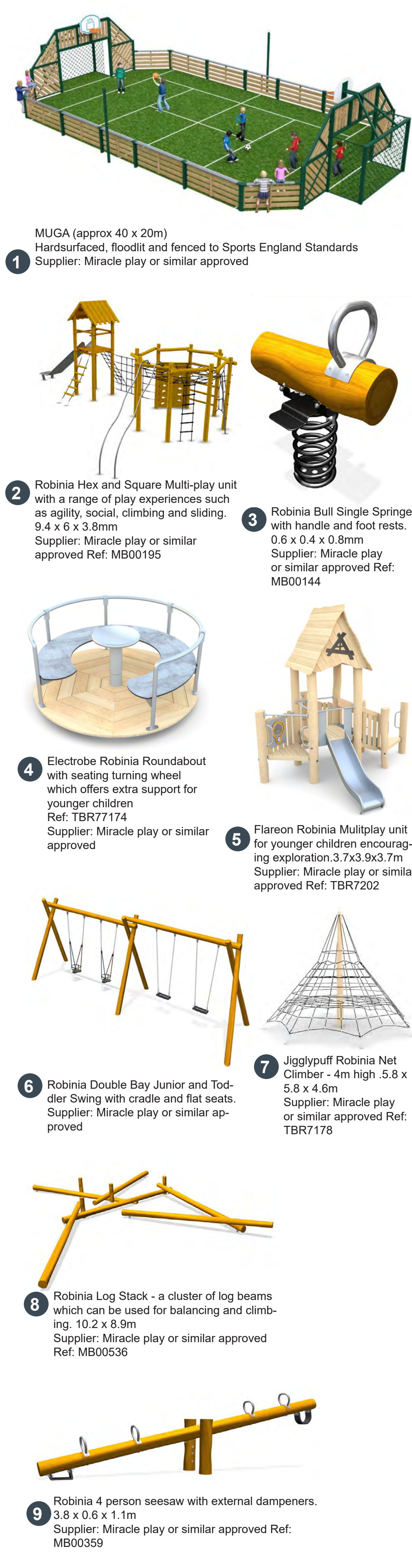
DATE: **13.09.2022**
SCALE: **1:2500 @ A1**
DRAWN: **AF**
CHECKED: **RTD**

BLOOR HOMES®

Drawing No. NW112-SL-1060 A

Common Infrastructure





LEGEND

EXISTING RETAINED TREES
Please refer to the Arboricultural Report for further details on species and condition and recommendations.

EXISTING RETAINED HEDGEROWS
Please refer to the Arboricultural and Ecology Reports for further details on species and condition and recommendations.

ROOT PROTECTION AREAS
Please refer to the Arboricultural Report for further details on species and condition and recommendations.

EXISTING VEGETATION TO BE REMOVED
Please refer to the Arboricultural Report for further details on species.

WMP
WILDFLOWER MEADOW PLANTING
Please refer to the planting schedule for details on species, quantities and nursery stock

MP
MARGINAL PLANTING
Please refer to the planting schedule for details on species, quantities and nursery stock

SP
SHRUB PLANTING
Please refer to the planting schedule for details on species, quantities and nursery stock

LANDFORM FEATURE
Please refer to the planting schedule for details on species, quantities and nursery stock

AMENITY GRASS
Grass areas to be seeded with or similar approved

BENCH
Please refer to the planting schedule for details on species, quantities and nursery stock

PLAY AREAS
Equipment with fall distances. Please to indicated drawings for details on play equipment

OUTDOOR GYM
Please to indicated drawings for details on gym equipment

ALLOTMENTS
Allotment area to be fenced, with parking and drop off area and water

PROPOSED TREE
Please refer to the planting schedule for details on species, quantities and nursery stock

NOTES:

All drawings to be read in conjunction with other technical reports such as ecology, arboricultural, highways and architectural layouts and drainage strategy.

P2	02.09.22	Amended layout	KLJ
P1	23.03.22	Initial Issue	KLJ
Rev	Date	Description	Initials
<div><div><div>pglalandscapearchitects</div></div><div><div>PGLA Ltd Landscape Architects Station Hop Offices 815 Stockport Road Manchester M19 3BS</div><div>t: 0161 238 9138 e: info@pgla.co.uk w: www.pgla.co.uk</div></div></div>			
<div><div>Client</div><div>BLOOR HOMES & VISTRY PARTNERSHIP</div><div>Project</div><div>LEIGHTON WEST</div><div>Description</div><div>COMBINED GREEN INFRASTRUCTURE FLOWERS LANE NEAP & MUGA</div></div>			
<div><div>Status</div><div>FOR PLANNING</div><div>Scale</div><div>1:200@A1</div><div>Job number</div><div>LEI2201-</div><div>Drawn</div><div>KLJ</div><div>Checked</div><div>PGG</div><div>Drawing number</div><div>GI02-P-01</div><div>Date</div><div>MAR 2022</div><div>Revision</div><div>P2</div></div>			
<div>Original size 100mm @ A1</div>			



- 

1 Torvill pull up bar
Supplier: Miracle Play or similar approved
Ref: SB6633
- 

2 Kingsbury cross trainer
Supplier: Miracle Play or similar approved
Ref: SB6846
- 

3 Bennet Hand wheel spinner
Supplier: Miracle Play or similar approved
Ref: SB6841
- 

4 Clancy pull down station
Supplier: Miracle Play or similar approved
Ref: SB6843
- 

5 Smith steo up box
Supplier: Miracle Play or similar approved
Ref: SB6649
- 

6 Larner Rowing Station
Supplier: Miracle Play or similar approved
Ref: SB6847
- 

7 Hill abs station
Supplier: Miracle Play or similar approved
Ref: SB6839
- 

8 Williams parallel bars
Supplier: Miracle Play or similar approved
Ref: SB6632
- 

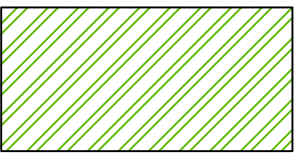
9 Taylor squat station
Supplier: Miracle Play or similar approved
Ref: SB6834
- 

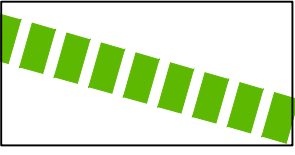
10 Kingsbury cross trainer
Supplier: Miracle Play or similar approved
Ref: SB6846
- 

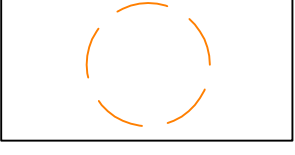
11 Kenny skier station
Supplier: Miracle Play or similar approved
Ref: SB6833
- 

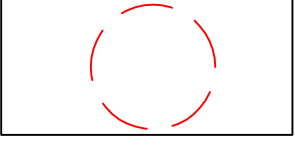
12 Radmilovic Air walker
Supplier: Miracle Play or similar approved
Ref: SB6832

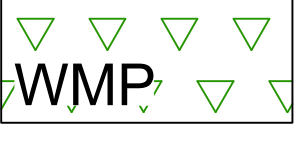
LEGEND


- 


EXISTING RETAINED TREES
Please refer to the Arboricultural Report for further details on species and condition and recommendations.
- 

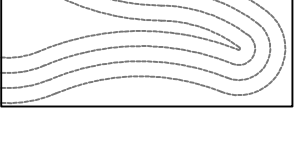
EXISTING RETAINED HEDGEROWS
Please refer to the Arboricultural and Ecology Reports for further details on species and condition and recommendations.
- 

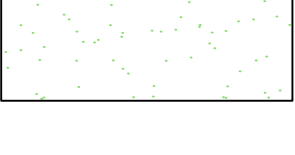
ROOT PROTECTION AREAS
Please refer to the Arboricultural Report for further details on species and condition and recommendations.
- 

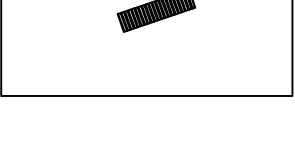
EXISTING VEGETATION TO BE REMOVED
Please refer to the Arboricultural Report for further details on species.
- 

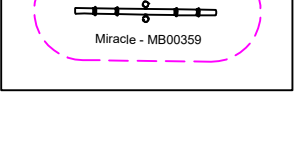
WMP
WILDFLOWER MEADOW PLANTING
Please refer to the planting schedule for details on species, quantities and nursery stock
- 

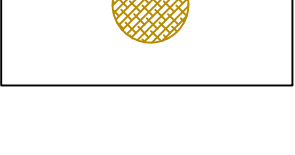
MP
MARGINAL PLANTING
Please refer to the planting schedule for details on species, quantities and nursery stock
- 

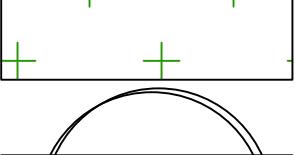
SP
SHRUB PLANTING
Please refer to the planting schedule for details on species, quantities and nursery stock
- 


LANDFORM FEATURE
Please refer to the planting schedule for details on species, quantities and nursery stock
- 

AMENITY GRASS
Grass areas to be seeded with or similar approved
- 

BENCH
Please refer to the planting schedule for details on species, quantities and nursery stock
- 

PLAY AREAS
Equipment with fall distances. Please to indicated drawings for details on play equipment
- 


OUTDOOR GYM
Please to indicated drawings for details on gym equipment
- 

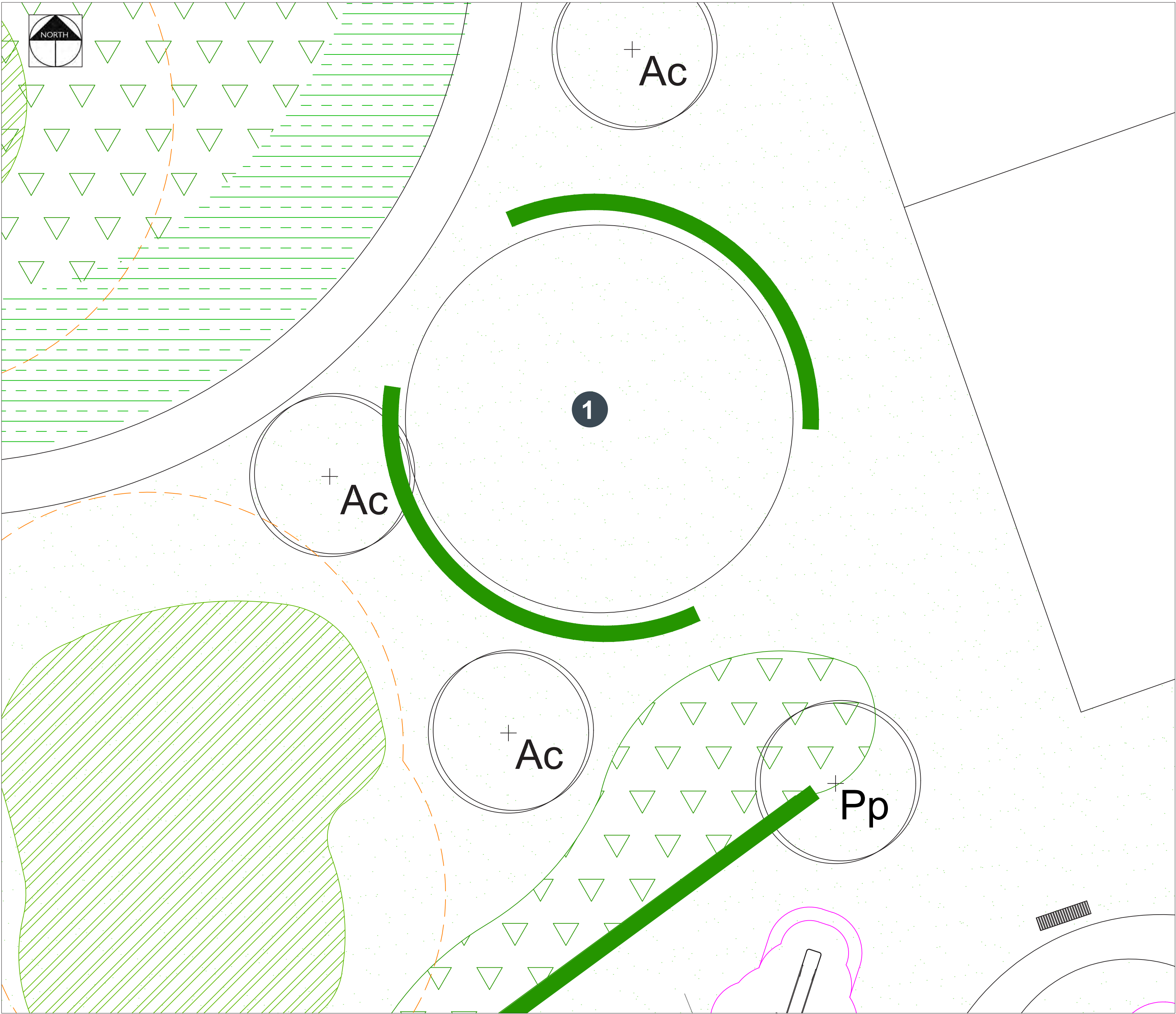
ALLOTMENTS
Allotment area to be fenced, with parking and drop off area and water
- 

PROPOSED TREE
Please refer to the planting schedule for details on species, quantities and nursery stock

All equipment to have the relevant safety surfacing in line with manufacturers recommendations.

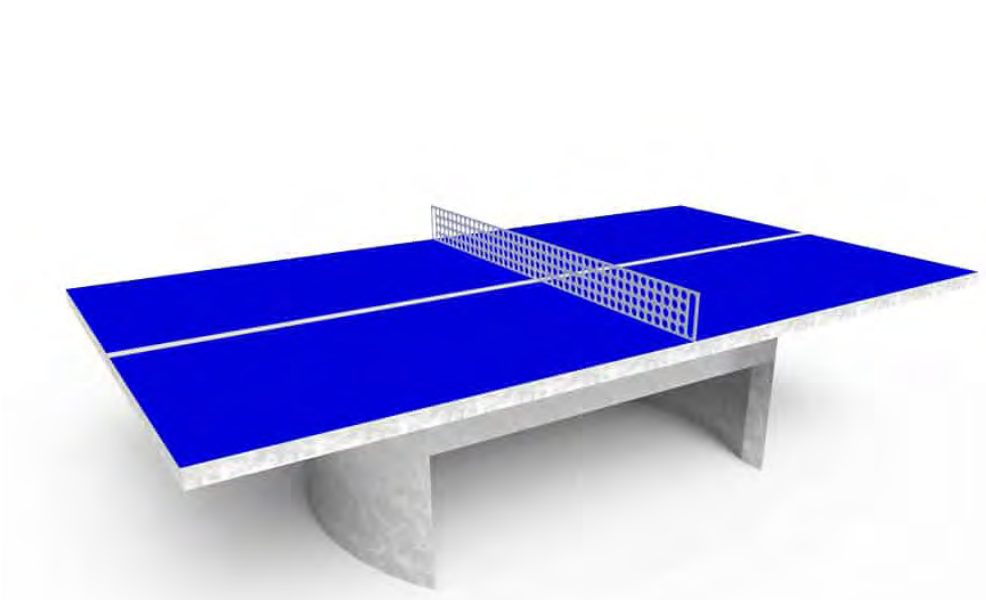
NOTES:
All drawings to be read in conjunction with other technical reports such as ecology, arboricultural, highways and architectural layouts and drainage strategy.

P1	23.03.22	Initial Issue	KLJ								
Rev	Date	Description	Initials								
<div></div> <p>PGLA Ltd Landscape Architects Station Hop Offices 815 Stockport Road Manchester M19 3BS</p> <p>t: 0161 238 9138 e: info@pgla.co.uk w: www.pgla.co.uk</p> <p>Client BLOOR HOMES & VISTRY PARTNERSHIP</p> <p>Project LEIGHTON WEST</p> <p>Description COMBINED GREEN INFRASTRUCTURE OUTDOOR GYM EQUIPMENT</p> <p>Status FOR PLANNING</p> <table><tr><td>Scale NTS</td><td>Drawn KLJ</td><td>Checked PGG</td><td>Date MAR 2022</td></tr><tr><td>Job number LEI2201-</td><td>Drawing number GI03-G-01</td><td>Revision P1</td><td></td></tr></table>				Scale NTS	Drawn KLJ	Checked PGG	Date MAR 2022	Job number LEI2201-	Drawing number GI03-G-01	Revision P1	
Scale NTS	Drawn KLJ	Checked PGG	Date MAR 2022								
Job number LEI2201-	Drawing number GI03-G-01	Revision P1									
Original size 100mm @ A1											



All play equipment to have the relevant safety surfacing in line with manufacturers recommendations. Fall distances indicated in pink

Teenage Area
Space adjacent to MUGA, playing fields and NEAP.
To contain seating area and activity such as table tennis or bespoke activity area.



Outdoor table tennis table
Supplier: Miracle play or similar approved



Seating and sheltered area



Bespoke activity area

LEGEND

- EXISTING RETAINED TREES
Please refer to the Arboricultural Report for further details on species and condition and recommendations.
- EXISTING RETAINED HEDGEROWS
Please refer to the Arboricultural and Ecology Reports for further details on species and condition and recommendations.
- ROOT PROTECTION AREAS
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NOTES:
All drawings to be read in conjunction with other technical reports such as ecology, arboricultural, highways and architectural layouts and drainage strategy.

P1	23.03.22	Initial Issue	KLJ
Rev	Date	Description	Initials



PGLA Ltd
Landscape Architects
Station Hop Offices
815 Stockport Road
Manchester
M19 3BS

t: 0161 238 9138
e: info@pgla.co.uk
w: www.pgla.co.uk

Client
BLOOR HOMES & VISTRY PARTNERSHIP
Project
LEIGHTON WEST
Description
**COMBINED GREEN INFRASTRUCTURE
LEIGHTON GREENWAY TEENAGE PLAY**

Status FOR PLANNING			
Scale	Drawn	Checked	Date
1:100@A1	KLJ	PGG	MAR 2022
Job number	Drawing number		Revision
LEI2201-	GI02-P-07		P1

Application No: 22/3158N

Location: LAND WEST OF DAVID WHITBY WAY, CREWE

Proposal: Construction of road, required to enable alternative access to Arriva Traincare site

Applicant: Network Rail Infrastructure Ltd

Expiry Date: 17-Nov-2022

SUMMARY

The application proposes the provision of an access road (850m) running off the western end of the existing spine road from David Whitby Way, through the employment site subject to application 23/0539N (submitted by Muse Developments Limited) and terminates at the Arriva Train Care depot to the northwest.

The access road provides access to a significant part of strategic allocation LPS2 - Basford East, Crewe of the CELPS for the delivery of employment use from David Whitby Way via the western end of the existing spine road. Its provision is therefore considered acceptable in principle.

It specifically provides an alternative, safe and improved access to the Arriva rail depot via David Whitby Way in accordance with the requirements of Policy LPS 2. The existing vehicular access serving the depot is via a "high risk" level crossing, and its removal will constitute a significant safety benefit.

The design and alignment of the access road is acceptable and consistent with that approved to serve the MUSE employment scheme under 23/0539N, incorporating pedestrian/cycle/infrastructure to ensure good connectivity within the employment site and beyond. No objections are raised by the CEC Highway Officer subject to a condition being attached requiring the closure of the existing access serving the rail depot prior to the commencement in use of the new access road, and a commuted sum being secured for the adoption of the road and maintenance of Suds.

Issues relating to trees, drainage/flood risk, amenity and landscaping have been addressed and are subject to conditions where necessary.

Ecological issues have been satisfactorily addressed particularly in respect to safeguarding habitat (including Basford Brook) and protected species subject to a series of planning conditions being imposed. The delivery of biodiversity net gain is achieved through the creation of off-site habitat secured through a S106 Agreement.

The scheme is therefore considered to be in accordance with development plan policies and economically, socially and environmentally sustainable.

RECOMMENDATION:

APPROVE, Subject to conditions and the prior completion of a S106 Agreement

DESCRIPTION OF SITE AND CONTEXT

The proposals relate to a large tract of greenfield land which forms part of the Basford East Strategic Allocation defined under CELPS Policy LPS 2. The Council (SPB - May 29th, 2024) resolved to grant planning approval (23/0539N – submitted by Muse Developments Ltd) for the siting of a significant employment development this part of allocation LPS 2 subject to the completion of a S106 Agreement.

The western boundary of the site adjoins the West Coast main railway line and the Arriva Train Care depot.

Mere Gutter and Basford Brook Local Wildlife Site (LWS) is located immediately to the north of the application site. This watercourse supports one of only a few remaining populations of White Clawed Crayfish in Cheshire. Basford Brook is also identified under Policy LC6 of the Weston and Basford Neighbourhood Plan as a Wildlife Corridor.

The adjoining area to the east has detailed approval (21/4434N) for a residential scheme of 325 dwellings which is now being implemented by Taylor Wimpey and forms part of the wider Basford East Strategic Site. A recently constructed spine road runs westward from the roundabout on David Whitby Way and serves the Taylor Wimpey residential scheme as well as enabling access to the future MUSE employment development (23/0539N).

A public right of way (Basford FP1) runs north/south through the application site.

DETAILS OF PROPOSAL

The application proposed to the provision of an access road (850m) running off the western end of the existing spine road from David Whitby Way, through the MUSE employment site and terminating at the Arriva Train Care depot to the northwest. The new access road will provide an alternative vehicular access to the rail depot and thereby replace the use of the Crewe Carriage Sidings Level Crossing which is identified by Network Rail as one of the highest risk crossings and proposed for closure.

The eastern section of the access road will be of the same design and alignment as that already approved to serve the Muse employment development (23/0539N).

Revised plans and additional information have been received during the application process in response to issues raised by the Council, including drainage and ecology. In particular, the drainage scheme has been revised to include the provision of a three treatment SuDS scheme which would discharge surface water via a detention basin to Basford Brook in the north western corner of the site.

RELEVANT HISTORY

With respect to the site itself

23/0539N - Full planning permission for Units 1-3 comprising a mix of Class B2 / E(g)(iii) light industrial / manufacturing and B8 warehousing & distribution uses and

ancillary areas, together with access, car parking provision, landscaping (including buffers, habitat parks, nature trails), sustainable drainage features (ponds, swales and raingardens), ecological enhancements (wet meadows, woodland planting, wildflower grassland), and other associated works and infrastructure proposed within the northern part of the site, within the curtilage of Units 1 - 3. Outline planning permission for the southern part of the site comprising a mix of Class E(g)(i) offices, B2 / E(g)(iii) light industrial / manufacturing and B8 warehousing & distribution uses, with all matters reserved including (access, appearance, landscaping, layout and scale) together with works and infrastructure associated with the southern part of the site - Resolved to Approve subject to completion S106 Agreement

With respect to sites within the LPS 2 Basford East, Crewe allocation

21/4434N - Reserved matters application proposing details of layout, appearance, scale and landscaping for the residential element (C3 use) of the outline development
15/1537N - Outline planning application (with all matters reserved) for a mixed-use development comprising residential use (Use Class C3) (up to 325 residential dwellings); employment use (Use Class B1), local centre comprising health centre and community facility (Use Class D1), food/non food retail (Use Class A1), public house/restaurant (Use Class A4/A3) and associated works including construction of a new access road with access from the Crewe Green Link Road South, creation of footpaths and provision of public open space and landscaping. Approved 7-Oct-2022

20/0615N - Deed of variation in connection with outline planning permission 15/1537N. Approved to Modify S106 27-Mar-2020

19/5934N - Approval of Reserved Matters following Outline Approval 14/4025N - Outline application for the erection of upto 490 residential dwellings and a primary school - 2000m2 (D1) a pumping station, substation, recreational open space, ecological mitigation area, internal access routes, ground modeling and drainage works, parking provision, footpaths, cycle routes, landscaping and associated works including details of access at the Basford East site Crewe
Approved 26- Nov-2020

19/2545N - Approval of all reserved matters following outline approval 15/1537N for the infrastructure works at Basford East. Approved 05-Nov-2019

19/3649N - Hybrid planning application for mixed use and residential development comprising; a) in full: the conversion, alteration and extension of the former mill and two farm buildings to business / professional services (Classes A2 and B1) and/or food and drink (Class A3) and/or non-residential community uses (Class D1) and/or leisure uses (Class D2), under Class V of Part 3 of Schedule 2 of the Town and Country Planning (General Permitted Development) (England) Order 2015 (as amended) with initial vehicular and pedestrian access from Mill Lane and permanent vehicular and pedestrian access from the future highway network serving the Basford East Strategic Site, and associated car parking and landscaping, and; b) in outline: residential development with vehicular access from the future highway network serving the Basford East Strategic Site and associated car parking and landscaping, and with all other matters reserved. (Crotia Mill) - NOT DETERMINED

19/2545N - Approval of all reserved matters following outline approval 15/1537N for the infrastructure works at Basford East. Approved 05-Nov-2019

19/0704N - Non-material amendment to approved application 15/1537N - Outline planning application (with all matters reserved) for a mixed-use development comprising residential use (Use Class C3) (up to 325 residential dwellings); employment use (Use Class B1), local centre comprising health centre and community facility (Use Class D1), food/non-food retail (Use Class A1), public house/restaurant (Use Class A4/A3) and associated works including construction of a new access road with access from the Crewe Green Link Road South, creation of footpaths and provision of public open space and landscaping. Approved 10-March-2019

16/2465N - Variation of Conditions 4, 5 and 6 on application 14/1366N - to fell additional trees as part of the Crewe Green Link Road Scheme. Approved 03-Nov-2016

15/3550N – Non-material amendment to 14/1366N - Dual carriageway road, known as the Crewe Green link Road (south) linking A500 with the A5020 and associated works. Approved 25-Aug-2015

15/1537N - Outline planning application (with all matters reserved) for a mixed-use development comprising residential use (Use Class C3) (up to 325 residential dwellings); employment use (Use Class B1), local centre comprising health centre and community facility (Use Class D1), food/non-food retail (Use Class A1), public house/restaurant (Use Class A4/A3) and associated works including construction of a new access road with access from the Crewe Green Link Road South. Approved 23-Dec-2016

14/4025N - Outline application for the erection of upto 490 residential dwellings and a primary school - 2000m2 (D1) a pumping station, substation, recreational open space, ecological mitigation area, internal access routes, ground modeling and drainage works, parking provision, footpaths, cycle routes, landscaping and associated works including details of access at the Basford East site Crewe. Approved 09-Feb-2016

14/1366N - Variation of condition 2 (plans) attached to planning application 12/4115N. Dual carriageway road, known as the Crewe Green Link Road (South) linking the A500 with the A5020 and associated works. Approved 06-Jun-2014

12/4115N - Dual carriageway road, known as the Crewe Green Link Road (South) linking the A500 with the A5020 and associated works. Approved 18-Jan-2013

POLICIES

Cheshire East Local Plan Strategy (CELPS)

LPS 2 - Basford East

MP1 - Presumption in Favour of Sustainable Development

PG 1 - Overall Development Strategy

PG 2 - Settlement Hierarchy

PG 7 - Spatial Distribution of Development

SD 1 - Sustainable Development in Cheshire East
SD 2 - Sustainable Development Principles
SE 1 - Design
SE 2 - Efficient Use of Land
SE 3 - Biodiversity and Geodiversity
SE 4 - The Landscape
SE 5 - Trees, Hedgerows and Woodland
SE 6 - Green Infrastructure
SE 7 - The Historic Environment
SE 8 - Renewable and Low Carbon Energy
SE 9 - Energy Efficient Development
SE 12 - Pollution, Land contamination and Land instability
SE 13 - Flood Risk and Water Management
CO 1 - Sustainable Travel and Transport
CO 2 - Enabling Business Growth Through Transport Infrastructure
CO 4 - Travel Plans and Transport Assessments
EG 1 - Economic Prosperity
EG 3 - Existing and Allocated Employment Sites
IN 1 - Infrastructure
IN 2 - Developer Contributions

Site Allocations and Development Policies Document (SADPD)

PG9 Settlement Boundaries
GEN1 Design principles
ENV1 Ecological network
ENV2 Ecological implementation
ENV3 Landscape character
ENV5 Landscaping
ENV6 Trees, hedgerows and woodland implementation
ENV7 Climate Change
ENV12 Air quality
ENV14 Light pollution
ENV15 New development and existing uses
ENV16 Surface water management and flood risk
ENV17 Protecting water resources
HER 1 Heritage assets
HER 4 Listed Buildings
HER 5 Registered parks and gardens
HER 8 Archaeology
HOU12 Amenity
INF 1 Cycleways, Bridleways and footpaths
INF3 Highways safety and access
INF9 Utilities

Weston and Basford Neighbourhood Plan 2015 - 2030

- Modified plan made on 20th February 2024

However, the Modified Neighbourhood Plan states that, "For the avoidance of doubt, the policies in the Plan do not cover the land at the major allocations at Basford West, Basford East and South Cheshire Growth Village."

Other Material Considerations

National Planning Policy Framework (NPPF)
National Planning Policy Guidance (NPPG)
Adopted SPDs

CONSULTATIONS

Environmental Protection: No objection. Recommend standard informatives are attached relating to hours of construction, dust management and unforeseen contamination.

Natural England: No objection

Historic England: No comments.

Network Rail: No objection.

HS2 Ltd: No objection. The High-Speed Rail link between Birmingham and Manchester (HS2 Phase 2) is cancelled.

CEC Strategic Highways Manager: No objection. Subject to;

- a condition requiring that on first use of the new access road serving the depot, the existing access is closed,
- a commuted sum of £110,000 for the adoption of the road and maintenance of the Suds by CEC secured by S106 Agreement

Public Rights of Way: No objection. Recommended that informatives are attached in respect of the unauthorised obstruction/diversion of the public footpath and variation to the Public Right of Way is approved in consultation with the Public Rights of Way team and the Highway Authority.

Cadent: No objection

Flood Risk Manager: No objection subject to a condition being attached requiring the submission and approval of the finalised drainage strategy.

Environment Agency: Object. Insufficient information provided in relation to the impact on White-clawed Crayfish and its habitat, water quality treatment or outfalls to Mere Gutter / Basford Brook.

Weston and Crewe Green Parish Council: Object, raising the following concerns;

Supplementary comments received in relation to amended proposals;

- This application is intertwined with 23/0539N. The complication arises around the fact that Network Rail are providing an essential part of the highway infrastructure which will allow 23/0539N to be implemented. This constitutes part of the access road off the central roundabout on David Whitby Way leading towards the West Coast Main Line beyond the Taylor Wimpey development.

- The Transport Assessment gives an assessment of the volume and type of vehicle which will use this central access road from David Whitby Way and states;

" Estimated daily usage for 137 personal vehicles. Estimated daily usage of 40 commercial vehicles. These include Fuel Tankers, Environmental Tankers, Flat beds and Containers including Refuse Lorries, Delivery Vans and Low Loader flatbeds for carrying wheelsets and bogies".

- The applicant adds that, *"the current usage for personal vehicles may increase, however given the improved access that will be available for pedestrians and cyclists and the delivery of new homes within the LPS 2 Development this usage could decrease."* The Parish Council has difficulty in quantifying and reconciling this statement to assess the volume of traffic which might ultimately end up entering and leaving this site.

- Proposed increase of traffic, particularly heavy vehicles, the problems created by the MUSE development will only be exacerbated to the extreme detriment of the amenities of the residents on this part of the new Wimpey estate.

- This proposal can only be made acceptable by a reduction in the scale of the MUSE proposal (23/0539N). Requested that the two applications (23/0539N and 22/3158N) are considered at the same time, given the impact that one has on the other.

Previous Comments:

Comments of the former Weston & Basford Parish Council were submitted on 31st August 2022

- This application is on land immediately to the west of the Taylor Wimpey housing site on Basford East which has only relatively recently received a detailed planning approval. The proposed road feeds out onto the central roundabout off David Whitby Way. The land over which this proposed road crosses is allocated for industrial use as part of the Basford East Strategic Development Area identified in the Local Plan.

- No proposal has yet been formally submitted for the development of this industrial site and in consequence neither the density nor layout of this site is currently known. The Parish Council is extremely concerned in relation to this application and in the absence of further clarification raises a strong objection. The location of the new road runs east/west across the site and then north alongside the main Basford rail sidings complex. The question must be asked as to the extent to which this will compromise the detailed layout of the industrial site along with proposed uses, but even more important no indication is given as to the volume, type or size of vehicles which will use this road.

- There are also amenity considerations here relating the Wimpey housing development and on the opposite side of David Whitby Way is the proposed Primary School with considerable pedestrian activity in the vicinity of David Whitby Way at this point.

- Further clarification is required of the intensity and type of traffic likely to use this proposed road along with information as to how this proposal will fit into the overall layout of the Basford East Development at this point. This seems to be yet another

example of the piecemeal and un co-ordinated way in which this Strategic Development Area is being developed.

OTHER REPRESENTATIONS

Representations have been received from Taylor Wimpey.

It is requested that consideration is given the following items;

- The drainage strategy with regards to discharge into Basford Brook. The proposals submitted are not reflective of the designs approved under the TW reserved matters scheme.
- Achievement of safe interface between the residential scheme and the highway work proposed through a Construction Management Plan.

OFFICER APPRAISAL

Key Issues

- Principle of development
- Highways
- Ecology
- Trees
- Landscape
- Amenity
- Flood Risk/Drainage

Principle of Development

The access road proposed by this application is essentially to provide an alternative and safe access to the Arriva rail depot via David Whitby Way. The existing vehicular access serving the depot is via a “high risk” level crossing, and its removal will constitute a significantly safety benefit. .

However, the access road also passes through, and would provide access to, a significant part of an allocation LPS2 - Basford East, Crewe of the CELPS for the delivery of employment use from David Whitby Way via western end of the existing spine road.

The justification of Policy LPS 2 importantly adds that, “The provision of employment development is the principal and overriding objective of the Basford East Site”. It is further recognised that Basford East is a gateway site into Crewe and presents the opportunity to create a high-quality employment led, vibrant and sustainable, mixed-use development with excellent links to Crewe and the M6 Motorway. Key to the site's delivery is the provision of Crewe Green Link Road South (David Whitby Way).

Site Allocation LPS 2 Basford East sets out the development plan policy for the site. This includes, that its development over the Local Plan Strategy period 2010 -2030 will be achieved through:

1. The delivery of up to 19 hectares of B1 Office Space, up to 5 hectares of B2 floor

space; to include the creation of a fourth-generation business park, with generous green infrastructure provision. The site is not considered to be suitable for B8 uses, due to highway constraints;

However, in consideration of planning application 23/0539N (MUSE) the Council's Highway Officer advised that the inclusion of B8 use within the employment site, is not unsuitable in transport terms as the nature and extent of anticipated vehicular movements generated by the employment development would not have an adverse impact on the local highway network either in terms of highway safety, traffic management issues or capacity. The design and alignment of the proposed access road is the same as that already approved to serve the MUSE employment scheme.

Policy LPS 2 also includes a series of Site-Specific Principles relating to the overall Basford East site. The following Issues which are considered relevant to the provision of the new access road and are addressed in following sections of the report;

- Necessary contributions to road infrastructure improvements
- Provide improved access from David Whitby Way through to the existing Rail Depot
- Provision of pedestrian and cycleway links with connections through to South Cheshire Growth Village LPS 8
- Achieve a quality of place and green setting for the employment park with the integration of safe and secure pedestrian and cycle routes
- Provision of appropriate noise mitigation measures

Highways and Accessibility

A new access road is proposed to serve the Arriva Train Care Depot connecting to the western end of the adopted Homes England spine road which links to the roundabout on David Whitby Way.

Traffic Impact

The current access to the maintenance depot is from a narrow access leading from Weston Road which includes a level crossing identified as one to be closed. The applicant has submitted information of traffic movements, the number of existing trips that access the site via the level crossing (which is to be closed) is 177 vehicles per day that would switch to using the new access road upon completion. This figure is split between 137 light vehicles and 40 HGV vehicles. It is not anticipated by the applicant that there would be a material increase in traffic generation by the Arriva depot site in the future and there is now no impact on the route associated with HS2.

The new access road usage will be shared between the existing Taylor Wimpey development the Arriva Depot site and also the Muse employment development (23/0539N), and the effects of all three developments are therefore considered.

An assessment of all three sites using the access road has been undertaken. The Highway Officer points out that the traffic generation of 177 daily trips of the depot is very small when compared to the predicted daily traffic generation of the Muse

development which has approximately 460 peak hour trips alone using the access road.

In capacity terms, the level of traffic generated by the depot rail application can easily be accommodated by a 7.3 industrial standard access road and traffic impact arising from these proposals is therefore considered acceptable.

The cumulative traffic impact of all the developments has been assessed at various junctions in the consideration of 23/0539N for the Muse development. This determined that given the significant scale of the Muse employment scheme and its predicted vehicle movements, off-site improvements at various junctions, including the David Whitby Way/A500 roundabout and the Meremoor Moss roundabout would be required to mitigate its impact.

However, given the modest level of traffic generation from the depot there is no requirement for off-site junction improvements to be secured through this application.

Accessibility

The Highway Officer advises that the design of the road is the same as the existing spine road which is a 7.3m industrial standard road. It has a 3m wide shared pedestrian/cycle path on the northern side and a 2m footpath on the southern side enabling connection to internal footways/cycleways the Muse scheme, and this then connects with a similar facility on the western side of David Whitby Way. There is a network of existing footways north of the Basford East Strategic Site towards Crewe and the shared footway/cycleway extends to Crewe Green roundabout.

It is important that developments are able to be accessed via pedestrian footways and that cycle facilities are provided, preferably on segregated paths.

There are a number of side-road access points along the road to serve the future Muse employment development (23/0539N). The Highway Officer advises that the submitted road design has also been subject to a stage 1 safety audit that has not raised any significant safety concerns to be addressed.

The submitted highway design is acceptable and there are no objections to the alignment and road construction. Significant surface water drainage/maintenance issues have been resolved during the course of the application to allow CEC to adopt the full extent of the road from the depot to the existing end of the adopted spine road.

A commuted sum of £110,000 to be secured via a S106 Agreement for the adoption of the road by CEC is required as part of this application.

Summary

The proposed main access road is of an acceptable industrial standard to serve the Arriva Train Care depot and is intended to be adopted by the Council.

The capacity of this 7.3m wide road is more than adequate to serve the predicted traffic generation by the depot and also these additional trips do not affect capacity at any of the junctions on David Whitby Way.

The new access road usage will ultimately be shared between the Muse employment development, Taylor Wimpey residential scheme and also the Rail Depot. The Highway officer has advised that an assessment of all three sites using the access road has been undertaken. The traffic generation equates to 460 peak hour trips for the MUSE scheme using the access road and a comparatively modest number of 177 daily trips would be generated by the Network Rail Depot.

In consideration of planning application 23/0539N, it was determined that the impact on the wider highway network from the Muse development (23/0539N) would be required to be mitigated through the implementation of a scheme for signalisation of the David Whitby Way/A500 roundabout, and also an A500/ A5020 access improvement contribution of £2.45M to be secured through a S106 Agreement.

However, in terms of this application, the highway impact solely arising from the use of the access road serving the depot is not considered material and therefore off-site improvements are not necessary and cannot be justified.

The Highway Officer raises no objection, subject to a condition being attached requiring that the closure of the excising access serving the depot prior to the commencement in use of the new access road. In addition, a commuted sum of £110,000 for the adoption of the road and maintenance of the SuDS by CEC Highways is required to be secured by S106 Agreement.

Ecology

There are various ecology matters to consider and these are broken down into the following subsections and assessed accordingly. Revised comments have been received from the Council's Ecologist in respect of updated information which has been submitted in respect of the Biodiversity Net Gain (BNG) assessment, surveys and mitigation reports.

Statutory Designated Sites

The application site falls within Natural England's SSSI impact risk zones associated with road schemes. I note that Natural England have been consulted on this application and raised no objection.

Non-statutory Designated Sites

The Mere Gutter and Basford Brook Local Wildlife Site (LWS) is located immediately to the north of the application site. Sites such as this receive protection through Local Plan Core Strategy Policy SE3. The LWS supports one of only a two surviving native white clawed crayfish populations in Cheshire. This species is sensitive to changes in water quality.

The Council's Nature Conservation Officer and the EA (Ecology) raised concerns that insufficient information had been provided to demonstrate that the drainage scheme would not prevent water quality deterioration of Basford Brook.

However, a revised drainage strategy has been submitted to minimise contamination of the Local Wildlife with surface water from the road being discharged into Basford

Brook via SuDS features. The scheme proposes a minimum three-treatment SuDS scheme including an attenuation pond which removes pollutants and contaminants prior to surface water being discharged to the brook. The Council's Ecologist advises that the proposed SuDS would be sufficient in addressing the potential risk of the LWS being affected by contamination arising from the proposed development.

In addition, the submitted drainage strategy specifies that headwalls for the surface water outfall will be set back from the brook with surface water discharging via new swales following natural flow paths to the brook. This ensures that disturbance to the brook is minimised.

The Council's Ecologist further advises that risks related to construction phase disturbance and contamination can be addressed through the implementation of a Construction Environmental Management Plan (CEMP). This should include fencing off 'no go' areas during the construction process and emergency spill response plan and dust control to minimise contamination. A condition is recommended requiring the submission and approval of a CEMP.

A further condition is recommended requiring that the drainage scheme including the SUDS components as detailed in the submitted Surface Water Technical Note and also the appointment of ecological clerk of works to supervise the installation of the outfalls and swales in the vicinity of Basford Brook.

Priority Woodland

There is an area of woodland that appears on the national inventory of Priority Woodland Habitat located north of the application site. Habitats of this type receive local plan protection and are a material consideration for planning.

However, the Council Ecologist advises that this woodland does not appear to meet the definition of this habitat type, so whilst still being of biodiversity value, is not likely to meet the definition of a priority habitat type. Nevertheless, the loss of this woodland would still be accounted for in the Biodiversity Net gain calculations.

Terrestrial invertebrates

A number of Priority Butterfly and Moth species have previously been recorded on site. No specific invertebrate survey has however been undertaken in support of the application. Priority Species of this type are a material consideration for planning.

Based on the information currently available much of the application site is of limited value for invertebrates being arable in nature. No suitable food plants were recorded during the submitted surveys for the three priority butterfly species listed as recorded on site. Consequently, the Council's Ecologist considers that habitats on the application site may not be suitable to support these species.

The species of the priority moths previously recorded is site is however unknown, so it is impossible to assess the impacts of the development upon these species, but

they may potentially be adversely affected by the loss of woodland and scrub habitats resulting from the development.

Hedgerows

The submitted Ecological Assessment advises that the proposed development would result in the loss of a section of hedgerow H1, which is Important under the Hedgerow Regulations. Hedgerow removal has been limited to that required for the introduction of the new access road, and subsequent removal of the level crossing to improve safe access to the Arriva depot site.

The submitted Biodiversity Net Gain report refers to the creation of 210m of new hedgerows to ensure that the proposed development delivers a net gain in respect of hedgerows. The Council's Ecologist advises that the biodiversity metric calculation addressed below can be used to determine whether sufficient hedgerow planting is being provided to compensate for that lost.

Ponds and Aquatic Invertebrates

The proposed development would result in the loss of one pond (Pond 3). A second pond (Pond 9) is located immediately adjacent to the proposed road is proposed to be retained and protected by a retaining wall. A condition requiring the implementation of a CEMP will safeguard this pond.

The survey of pond 3 for aquatic invertebrate was undertaken when the pond was dry, The Council's Ecologist advises that the potential of pond 3 to support priority aquatic invertebrate species cannot therefore be fully discounted, and it can only be concluded that none were likely to be present at the time of the survey. No replacement ponds are currently proposed on site but are however proposed to be delivered at an offsite location.

Great Crested Newts

This protected species is known to utilise ponds within the broad locality of the application site. Updated surveys have identified a population of this species at a single pond on site.

In the absence of mitigation, the proposed development would result in an adverse impact upon this species as a result of the loss of terrestrial habitat, habitat fragmentation and the risk of newts being killed during the construction phase.

The EC Habitats Directive 1992 requires the UK to maintain a system of strict protection for protected species and their habitats. The Directive only allows disturbance, or deterioration or destruction of breeding sites or resting places:

(a) in the interests of public health and public safety, or for other imperative reasons of overriding public interest, including those of a social or economic nature and beneficial consequences of primary importance for the environment, and provided that there is;

- (b) no satisfactory alternative and
- (c) no detriment to the maintenance of the species population at favourable conservation status in their natural range

The UK has implemented the Directive in the Conservation (Natural Habitats etc) Regulations 2010 (as amended) which contain two layers of protection (i) a requirement on Local Planning Authorities ("LPAs") to have regard to the Directive's requirements above, and (ii) a licensing system administered by Natural England and supported by criminal sanctions.

Policy SE 3 of the CELPS states that development which is likely to have a significant impact on a site with legally protected species will not be permitted except where the reasons for or the benefits of the development outweigh the impact of the development. Circular 6/2005 advises LPAs to give due weight to the presence of protected species on a development site to reflect EC requirements. "This may potentially justify a refusal of planning permission."

The NPPF advises LPAs to protect and enhance biodiversity: if significant harm resulting from a development cannot be avoided (through locating on an alternative site with less harmful impacts) or adequately mitigated, or as a last resort, compensated for, planning permission should be refused.

Natural England's standing advice is that, if a (conditioned) development appears to fail the three tests in the Habitats Directive, then LPAs should consider whether Natural England is likely to grant a licence: if unlikely, then the LPA should refuse permission: if likely, then the LPA can conclude that no impediment to planning permission arises under the Directive and Regulations.

In terms of the Habitat Directive tests;

- The proposed development is of overriding public interest. The proposed access road would provide improved alternative vehicular access to the Arriva rail depot and enable the closure of its existing narrow access over an identified "high risk" level crossing. In addition, the site forms part of the Basford East Strategic Allocation and the access road would also enable the delivery of significant employment opportunities and economic benefits for Crewe - supporting the overall aims of the local plan strategy and the important contribution made by this strategic site.
- There are no suitable alternatives in providing improved access to the rail depot from the Crewe Green Link road (David Whitby Way) as required under Clause (a) Site Specific Principle of development of CELPS allocation LPS 2 Basford East, Crewe. In addition, the road will provide suitable access for this large Strategic Site and for which there is no satisfactory alternative.
- In order to address the potential, impacts of the proposed development upon this species, the applicant intends to enter the development in Natural England's District Level Licencing scheme and has provided confirmation that the development has been accepted onto the scheme in principle.

The Council's Ecologist advises that entry into the district licencing scheme would be sufficient to maintain the favourable conservation status of the species. A condition

is therefore recommended requiring entry of the proposed development into Natural England's District Level licencing scheme prior to the commencement of development,

As a result, the proposed development would comply with the relevant policies of the Development Plan in terms of the impact upon protected species.

Common Toad

This priority species is known to be present on site. The Council's Ecologist advises that the development is likely to result in an adverse impact upon this species as a result of the loss of low-quality terrestrial habitat, habitat fragmentation and mortality occurring as a result of the operation of the road.

Badgers

A badger survey has been submitted in support of the application. Whilst badgers are active on site no active setts were recorded within suitable habitat affected by the proposals. Based upon the current status of badgers on site the proposed development will not have a direct impact upon a sett but would have a minor impact as a result of the loss of an area of suitable foraging habitat.

As the status of badgers on a site can change in a short timescale it is recommended that condition is attached requiring an updated badger survey to be undertaken with a report and mitigation strategy submitted prior to the commencement of development.

Bats

No evidence of roosting bats was recorded in associated with trees on site that would require removal to facilitate the proposed development. The proposed development is therefore unlikely to directly affect roosting bats.

The submitted Ecological Assessment identifies the loss of broad-leaved woodland, Pond P3 and the section of hedgerow H1 as resulting in a loss of moderate suitability bat foraging habitat. As discussed above it must be ensured that these losses are either avoided or as a last resort adequately compensated for. Whether adequate compensation is provided can be assessed through the Biodiversity Metric discussed below.

A lighting scheme has been submitted in support of the application. The Councils Ecologist advises that no lighting is proposed where the road enters the retained priority woodland in the north. Much of the proposed replacement hedgerow planting would however be affected by lighting and potentially deter foraging and commuting bats, although it is accepted that CEC Highway require the road to be lit.

Water Vole and Otter

Otters are likely to be present on the brook on at least a transitory basis. No suitable features for shelter were however recorded.

Whilst water voles have previously been present on Basford Brook this population appears to have been lost some time ago. This species is not reasonably likely to be affected by the proposed development.

Brown Hare and Hedgehog

Both of these priority species have been previously recorded within the broad vicinity of the application site. Whilst no evidence of these species was recorded on site, there remains a possibility that they may be present. The Councils Ecologist advises that the proposed development would result in an adverse impact upon this species, if present, due to the loss of suitable habitat and the risk of animals being harmed during the construction phase and road traffic collisions.

The potential impact on hedgehogs could be reduced through the retention of the existing woodland.

Reptiles

Slow worm is rare in Cheshire East and its presence is sufficient for a site to be selected as a Local Wildlife site. Slow worms are known to be present on site. The species was recorded during surveys undertaken in respect of application 23/0539N (Muse Developments).

The Councils Ecologist advises that considering the rarity of this species in Cheshire, the scheme should avoid impacts upon this species. The revised Slow Worm mitigation strategy includes a methodology for removal and exclusion of slow worm from the development footprint. A condition is therefore recommended that the development is undertaken in accordance with the submitted Reptile Mitigation Strategy.

Breeding Birds

A standard condition is recommended to safeguard nesting birds.

Biodiversity Net Gain

In accordance with Local Plan policy SE3(5) all development proposals must seek to lead to an overall enhancement for biodiversity. In order to assess the overall loss/gains of biodiversity an assessment undertaken in accordance with the Defra Biodiversity 'Metric' has been submitted in support of this application.

The metric calculation as submitted shows the proposed development, and associated habitat enhancement works outside the red line of the application, would deliver a biodiversity net gain as required by Local Plan policy. This meets the requirements of SADPD Policy ENV2, as opposed to the mandatory BNG requirements that came into effect in February 2024 and to which this application is exempt as it was submitted in August 2022.

The BNG metric shows that the proposed development would achieve a net gain for both area-based habitats and hedgerows. The submitted BNG report proposes that

BNG can be achieved through additional habitat creation either on other Network Rail land or on land located between the employment park and the A500.

A section 106 agreement would be required to secure the offsite biodiversity measures including the submission of a habitat creation method statement and 30-year monitoring and management plan.

A planning condition is required to secure the submission and implementation of a habitat creation method statement and a 30-year habitat management plan and monitoring plan for the on-site works.

This planning application provides an opportunity to incorporate features to increase the biodiversity value of the development in accordance with Local Plan Policy SE 3. A further condition is recommended of proposals for ecological enhancement including features for nesting birds and brash/deadwood piles.

Summary

In summary, subject to the completion of a S106 Agreement to secure off-site habitat to deliver Biodiversity net gain, and a series of planning conditions to protect species and safeguard/enhance habitat (including Basford Brook) the Council's Ecologist raises no objections to the development.

Trees

The application is supported by an Arboricultural Report & Impact Assessment. The survey confirms the presence of 7 individual and 1 group of moderate quality B Category trees, 19 individual and 9 groups of low-quality C Category trees and 1 poor quality U Category tree. Of these, 1 individual and 2 groups of moderate quality trees and 8 individual and 2 groups of low-quality trees have been identified as requiring removal to accommodate the project.

The Forestry Officer advises that the extent of tree cover required to be removed essentially comprises of young to early mature trees and although is considered quite significant, it is not afforded any statutory protection, nor is it widely visible from any public vantage points. As advised by the Council's Ecologist the area of tree cover affected does not appear to meet the definition of the Priority Woodland Classification, and it is considered that sufficient information has been provided to classify the woodland as non-priority due to the woodland containing low species diversity.

The Arboricultural report has quantified the losses that would be necessary to accommodate the road and drainage scheme (SUDS). The overall impact of the proposal is considered to result in a slightly adverse, but not significant impact on the wider amenity of the area. While any tree loss is regrettable, it is accepted that provision could be made to secure the necessary levels of replacement planting in accordance with CELPS Policy SE5. As set out below landscaping is proposed inside the site boundary to compensate for the tree loss which is proposed, although the Forestry Officer considers that ideally further replacement planting off-site should be provided as well as meeting BNG requirements.

The arboricultural report confirms the position of all other retained trees and vegetation, and a tree protection scheme and method statement to be adhered to throughout any approved construction period will be required. The Forestry Officer raises no objection to the road scheme, subject to conditions being attached requiring the submission of a scheme of tree protection measures and Arboricultural Method Statement to ensure the long-term retention of the trees.

Landscape

A key landscape requirement of LPS 2 is the provision of green infrastructure including new planting, the creation of tree lined boulevards, provision of green spaces and retention of existing trees and hedgerows.

Although the development will involve some loss of trees and also the removal of hedgerow to enable the new access road to link to the western end of the existing spine road, this will be largely compensated by the proposed planting scheme. The landscape master plan indicates the provision of a “green corridor” alongside the access road, comprising of a significant number of street trees lining the route combined with native hedgerow planting. In addition, naturalistic tree groups and native planting, including wetland meadow, are shown to enhance the proposed SUDs area.

The Landscape Officer advises that overall, the landscape proposals are acceptable at this stage. However, a condition is recommended to be imposed requiring finalised details of the landscape scheme and planting specifications be submitted for approval.

Amenity

SADPD Policy HOU 12 (Amenity) states that new development should not be permitted if it is deemed to cause unacceptable harm upon neighbouring amenity such as from visual intrusion or noise and disturbance.

Residential properties of the approved Taylor Wimpey (TW) development (21/4434N) will front onto the spine road leading from the roundabout on David Whitby Way. The approved TW layout ensures that proposed dwellings are well set back behind buffer planting along the central spine road, given this would also serve as the main access to the adjoining employment site to the west (and the rail depot). In addition, roadside tree planting the landscape buffer areas, will provide a green corridor into the development of depths of between around 7m to 11m in depth.

The numbers of vehicle movements which would access the rail depot via the new road from David Whitby Way, would however be low, and not therefore result in levels of noise or disturbance harmful to the amenities of dwellings of the TW development. The Council’s Environmental Health Officer has raised no objections and noise mitigation measures are not required to be secured through this application.

However, in support of the Muse employment scheme (23/0539N) , a noise impact assessment was submitted and assessed by the EHO. In terms of traffic noise associated with vehicular movements generated by the employment development the report recommended the provision of mitigation in terms of an upgraded specification of double glazing and trickle vents for specified dwellings of the TW scheme adjacent to the access road, as well as additional 1.8 m high screen fencing to several plots to provide necessary acoustic screening from the road.

The EHO and TW considered the noise assessment and its recommended mitigation measures acceptable. Importantly, TW confirmed in its representations to application 23/0539N that, "Muse have confirmed that they will cover the reasonable costs for Taylor Wimpey in relation to the mitigation required".

Construction activities associated with the road scheme have the potential to result in noise and disturbance to neighbouring properties. To mitigate these impacts on future occupiers of the neighbouring TW scheme, a condition is recommended requiring the submission and approval of a Construction Environment Management Plan (CEMP) prior to the commencement of development.

Flood Risk/Drainage

A revised drainage scheme (SUDS) has been submitted, given the need to safeguard White Clawed Crayfish within Basford Brook.

As set out in the Ecology section of the report above, and notwithstanding the concerns raised by the EA, the Council's Ecologist advises that the proposed Suds scheme would be sufficient in addressing the potential risk of the LWS being affected by contamination arising from the proposed development and ensures that disturbance of the brook is minimised. A further condition is recommended requiring the provision of Construction Environmental Management Plan (CEMP) to be submitted to safeguard the Mere Gutter and Basford Brook LWS throughout the construction phase.

Basford Brook is designated as a 'main river' by the Environment Agency. Most of the site is located in Flood Zone 1 (lowest area of flood risk). An area in the north of the site, adjacent to Basford Brook, lies in Flood Zone 2 and 3.

Surface water would be dealt with by appropriate SuDs techniques as set out the Sustainable Drainage Systems Supplementary Planning Document (SuDS SPD). In particular, the proposed drainage strategy includes a range of SuDS features such as surface water passing through a filler trench, vortex separators swales and an attenuation pond prior to discharging to Basford Brook.

As Basford Brook is considered a 'sensitive watercourse' due to the presence White Clawed Crayfish, a "three-tiered" SuDS system is importantly proposed in accordance with the SuDS SPD. This ensures that enhanced water quality improvement is achieved by surface water passing through the suds systems before discharge into Basford Brook.

The Council's Flood Risk Manager has raised no objections in principle to the proposed Drainage Strategy. Although detailed issues are required to be addressed in respect of the final design of elements of the drainage system, and condition is therefore recommended to be imposed.

The application proposals are therefore deemed to adhere with Policy SE13 of the CELPS and Policy ENV 16 of the SADPD.

SECTION 106

A Section 106 Agreement is required to secure the following:

- A commuted sum of £110,000 for the adoption of the new road and maintenance of the SuDS by CEC Highways
- To secure the offsite habitat creation measures, including the submission of a habitat creation method statement and 30-year monitoring and management plan, to ensure the development deliver the required Biodiversity Net Gain (BNG)

CIL REGULATIONS

In order to comply with the Community Infrastructure Levy (CIL) Regulations 2010, it is necessary for planning applications with legal agreements to consider the issue of whether the requirements within the S106 satisfy the following:

- a) Necessary to make the development acceptable in planning terms;
- a) Directly related to the development; and
- c) Fair and reasonably related in scale and kind to the development.

It is considered that the contributions required as part of the application are justified meet the Council's requirement for policy compliance. As set out above, all elements are necessary, directly relate to the development and are fair and reasonable in relation to the scale and kind of development. On this basis the scheme is compliant with the CIL Regulations 2010.

CONCLUSIONS

The access road also passes through and would provide access to a significant part of allocation LPS2 - Basford East, Crewe of the CELPS for the delivery of employment use from David Whitby Way via the western end of the existing spine road. Its provision is therefore considered acceptable in principle.

In addition, the access road will specifically provide an alternative, safe and improved access to the Arriva rail depot via David Whitby Way in accordance with the requirements of Policy LPS 2 . The existing vehicular access serving the depot is via a "high risk" level crossing, and its removal will constitute a significant safety benefit.

The design and alignment of the access road is acceptable and consistent with that approved to serve the Muse employment scheme under 23/0539N, incorporating pedestrian/cycle/infrastructure to ensure good connectivity within the employment site and within Basford East beyond. No objections are raised by the Highways Officer subject to a condition being attached requiring the closure of the existing access

serving the rail depot prior to the commencement in use of the new access road and a commuted sum being secured for the adoption of the road and maintenance of Suds.

Issues relating to trees, drainage/flood risk, amenity and landscaping have been addressed and are subject to conditions where necessary.

Ecological issues have been satisfactorily addressed particularly in respect to safeguarding habitat (including Basford Brook) and protected species subject to a series of planning conditions being imposed. The delivery of biodiversity net gain is achieved through the creation of off-site habitat secured through a S106 Agreement.

On this basis, the proposal is for sustainable development which would bring environmental, economic and social benefits and is therefore considered to be acceptable in the context of the Development Plan, and advice contained within the NPPF.

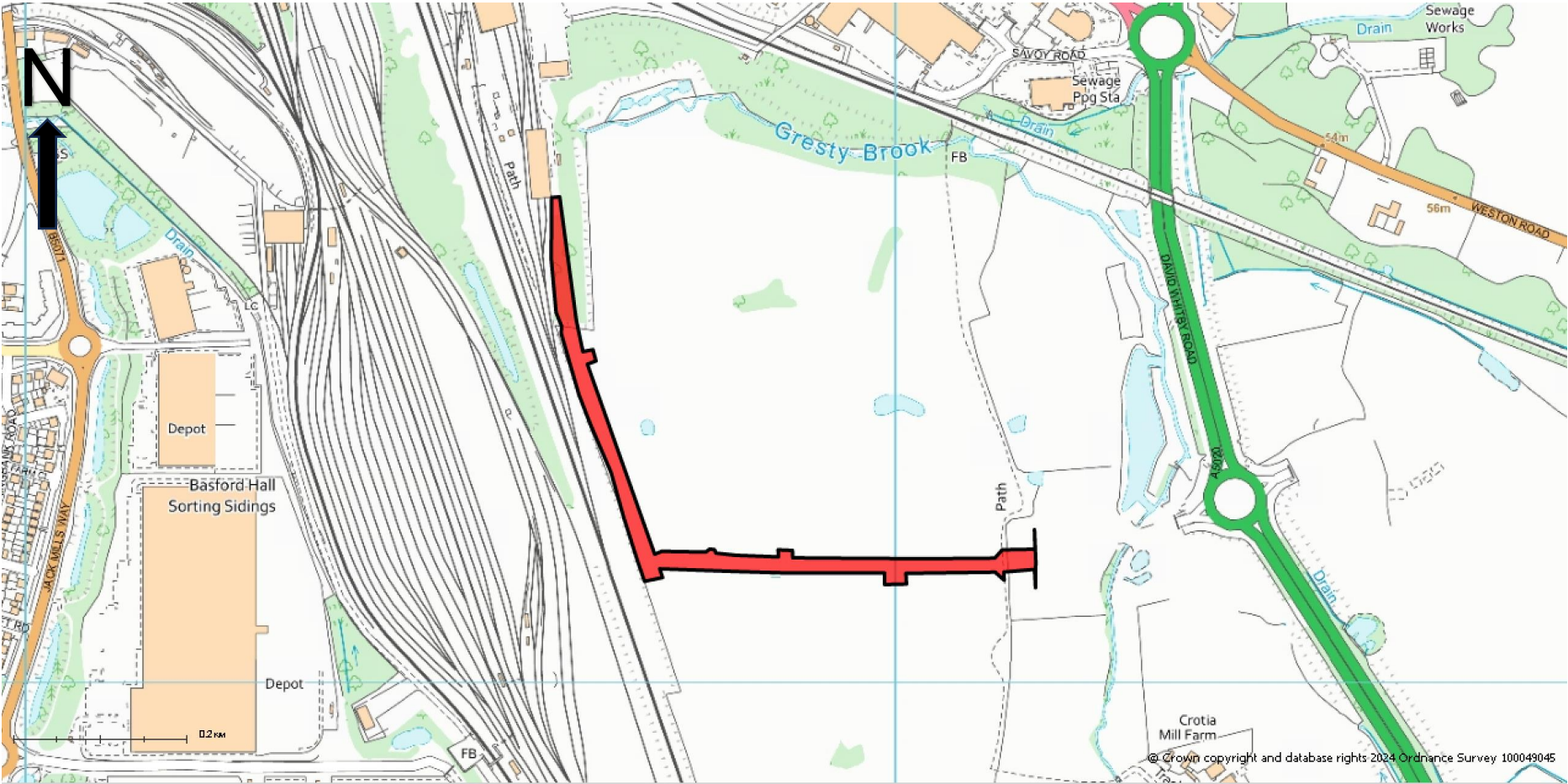
RECOMMENDATION: APPROVE, Subject to conditions and the prior completion of a S106 Agreement to secure the following:

S106	AMOUNT/REQUIREMENT	TRIGGER
Commuted Sum for adoption of the new road and maintenance of SuDS by CEC Highways	£110,000	Prior to commencement in use of road
<u>BNG</u> Details of offsite biodiversity measures including the submission of a habitat creation method statement and 30-year monitoring and management plan, and biodiversity metric	Submission of BNG measures, habitat creation method statement and 30-year monitoring and management plan, and biodiversity metric.	<ul style="list-style-type: none"> - Submit details for approval prior to commencement. - Delivery of habitat creation in the next appropriate season following commencement

AND subject to the following conditions;



1. Commencement of development (3 years)
2. Development in accordance with approved plans
3. Submission of details of landscaping scheme
4. Implementation of landscaping scheme
5. Submission of Arboricultural Method Statement prior to commencement of development

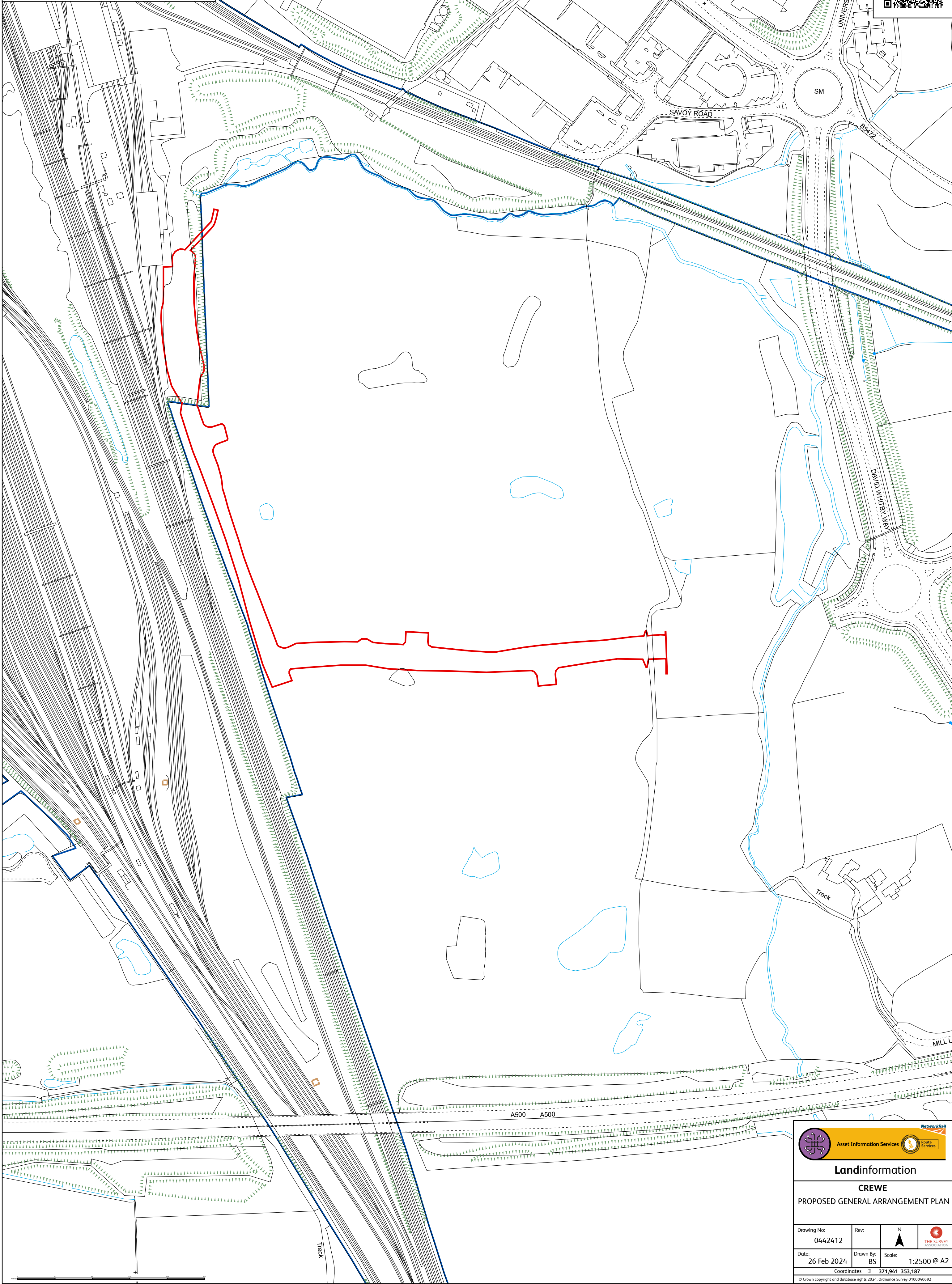
6. Submission of a scheme for the protection of trees prior to commencement of development
7. Details of Levels
8. Prior to commencement of development submission and approval of the finalised surface water drainage strategy (SUDS)
9. Closure of existing access using the Arriva Train depot on commencement in use of new road access.
10. Submission of a CEMP to include measures to safeguard the Mere Gutter and Basford Brook LWS and to safeguard residential amenity.
11. Drainage scheme to include specified SUDS treatments and appointment of ecological clerk
12. Entry of the proposed development into Natural England's District Level licencing scheme prior to commencement of works on site.
13. Updated badger survey prior to commencement of development
14. Safeguarding of nesting birds.
15. Submission and implementation of habitat creation method statement, and 30-year habitat management and monitoring plan for on-site habitat works.
16. Submission and Incorporation of features for ecological enhancement.
17. Implementation of Reptile Mitigation Measures.


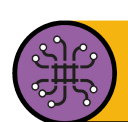


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22/3158N
LAND WEST OFF DAVID
WHITBY WAY
CREWE




 New Road ~ 24360m²
 Network Rail Ownership





Landinformation

CREWE
PROPOSED GENERAL ARRANGEMENT PLAN

Drawing No: 0442412	Rev:	 N	
Date: 26 Feb 2024	Drawn By: BS		
Coordinates  371,941 353,187			

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PLANTING NOTES:

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- Plants shall be first class examples of their species or variety, free from all pests and diseases, with good fibrous root systems and materially undamaged (refer to relevant sections of BS3936 Parts 1-4 'Specification of Nursery Stock').
- All planting is to be in general compliance with BS4428: 1989 'Code of Practice for general landscape operations (excluding hard surfaces)'.
- Planting areas in made ground to have a min depth of 450mm good quality topsoil, which complies with BS 3882. The topsoil should not be contaminated with subsoil, rubbish, rubble or any other materials that may be detrimental to plant growth. When storing the topsoil, compaction and contamination should be prevented by covering and not allowing construction plants to pass over it. It should also be handled in the driest conditions possible and not after a heavy rainfall. When spreading the topsoil do not firm, consolidate or compact it. Tip and grade to approximate levels in one operation with minimum of trafficking by plant. Once spread all shrub beds need to be cultivated to a depth of 200mm to break up any clods of earth, ready for planting.
- Native Hedge mix:
 - Area to be treated with a non-residual glyphosate based herbicide prior to planting.
 - Notch plant the bare root whips into the ground and heal into place.
 - Protect shrubs with a spiral bound biodegradable shrub guard and secure in place with timber stake.
 - Areas to be mulched with medium grade bark mulch.
- All trees are to be staked and secured as follows:
HS and SS (12-14cm and 10-12cm) - single timber stake using 10cm diameter short stakes (at 600mm above ground level, with a rubber tree tie)
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 - HS 700 x 700 x 600mm deep.
 - SS 600 x 600 x 500mm deep
- The contractor must ascertain for himself/herself the exact location of underground services and maintenance points.
- Water all plants and trees on the same day of planting and there after as necessary (min once a week in periods of drought) to ensure the establishment of the plants during the 12months defects
- Seeded areas to be cultivated to a depth of 150mm prior to seeding. Area to be sprayed with a non-residual herbicide.
Wet Meadow Seed Mix to be sown onto subsoil with a skim of topsoil to create a tilth which will aid germination. Wet Meadow Seed Mix not to be sown into waterlogged soil as seed may rot.

PLANT SCHEDULE					
TREE PLANTING		Spec	Height (m)	Type	Total
Bp	Betula pendula	10-12cm girth	3-3.5m	SS	Rootball
Sa	Sorbus aria	12-14cm girth	3.5-4m	HS	Rootball
Tc	Tilia cordata	12-14cm girth	3.5-4m	HS	Rootball
Ac	Acer campestre	10-12cm girth	3-3.5m	SS	Rootball
Fs	Fagus sylvatica	12-14cm girth	3.5-4m	HS	Rootball
Cb	Carpinus betulus	12-14cm girth	3.5-4m	HS	Rootball
NATIVE HEDGEROW MIX		Spec	Size (cm)	Density	Total
-	Viburnum opulus	Whip	80-100	5 per l.m.	BR
-	Cornus sanguinea	Whip	80-100	5 per l.m.	BR
-	Ilex aquifolium	3L	40-60	5 per l.m.	BR
-	Rosa canina	Whip	80-100	5 per l.m.	BR
-	Crateagus monogyna	Whip	80-100	5 per l.m.	BR
-	Corylus avelana	Whip	80-100	5 per l.m.	BR









Native Hedgerow Mix (452 Lm.)
226Nr Viburnum opulus (10%)
452Nr Cornus sanguinea (20%)
226Nr Ilex aquifolium (10%)
226Nr Rosa canina (10%)
904Nr Crataegus monogyna (40%)
226Nr Corylus avellana (10%)

Native Hedgerow Mix (21 Lm.)
215Nr Viburnum opulus (10%)
430Nr Cornus sanguinea (20%)
215Nr Ilex aquifolium (10%)
215Nr Rosa canina (10%)
860Nr Crataegus monogyna (40%)
215Nr Corylus avellana (10%)

Native Hedgerow Mix (319 Lm.)
160Nr Viburnum opulus (10%)
319Nr Cornus sanguinea (20%)
160Nr Ilex aquifolium (10%)
160Nr Rosa canina (10%)
638Nr Crataegus monogyna (40%)
160Nr Corylus avellana (10%)

LEGEND

SOFT LANDSCAPE

- **Existing Woodland to be Retained**
Approximate extents of existing woodland to be retained. To be retained in line with BS5837:2012.
- **Existing Trees, Scrub and Hedgerow to be Retained**
Approximate extents to be retained in line with BS5837:2012.
- **Existing Trees, Scrub and Hedgerow to be Removed**
Approximate extents to be removed, outside of bird nesting season
- **Proposed Native Hedgerow**
To be planted as 80-100cm BR whips at 5 plants per lin.m. To consist of at least 5 native shrub species and provide opportunities for nesting and foraging.
- **Proposed Street Trees**
Trees to comprise 12-14cm girth Heavy Standards with 2m clear stem. To be native species e.g. Sorbus aria.
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Trees within swale and open space 10-12cm girth Selected Standard with 2m clear stem. To consist of locally native species.
- **Proposed Wet Meadow Seed Mix**
To be WFG9 Wetland and Ponds by Germinal or similar approved sown at 4g per sq.m
- **Proposed Species Rich Grassland**
To be WFG20 by Germinal or similar approved sown at 4g per sq.m

See drawing 5246.01 for plant schedule

Rev B - Adjustments to red line boundary
GBS 29/02/2024

Rev A - Adjustments to drainage and woodland description
GBS 10/05/2023



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W: www.dep.co.uk

Client
J. Murphy & Sons Limited

Project
Arriva Train Care Access Road

Description
Masterplan incl. Planting (Sheet 1 of 3)

Status
Preliminary

Scale	Drawn
1:500@A1	GBS(TR)
Job number	Drawing number
5246	01
Date	Revision
Dec. '23	B

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3. All planting is to be in general compliance with BS4428: 1989 'Code of Practice for general landscape operations (excluding hard surfaces)'.

4. Planting areas in made ground to have a min depth of 450mm good quality topsoil, which complies with BS 3882. The topsoil should not be contaminated with subsoil, rubish, rubble or any other materials that may be detrimental to plant growth. When storing the topsoil, compaction and contamination should be prevented by covering and not allowing construction plants to pass over it. It should also be handled in the driest conditions possible and not after a heavy rainfall. When spreading the topsoil do not firm, consolidate or compact it. Tip and grade to approximate levels in one operation with minimum of trafficking by plant. Once spread all shrub beds need to be cultivated to a depth of 200mm to break up any clods of earth, ready for planting.

5. Native Hedge & Shrub mix:
i.) Area to be treated with a non-residual glyphosate based herbicide prior to planting.
ii.) Notch plant the bare root whips into the ground and heal into place.
iii.) Protect shrubs with a spiral bound biodegradable shrub guard and secure in place with timber stake.
iv.) Areas to be mulched with medium grade bark mulch.

6. All trees are to be staked and secured as follows:
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
8. The contractor must ascertain for himself/herself the exact location of underground services and maintenance points.


9. Water all plants and trees on the same day of planting and there after as necessary (min once a week in periods of drought) to ensure the establishment of the plants during the 12months defects


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
LEGEND


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Rev B - Adjustments to red line boundary
GBS 29/02/2024

Rev A - Adjustments to drainage and woodland description
GBS 10/05/2023



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Description
Masterplan incl. Planting (Sheet 2 of 3)

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Drawn
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5246 01

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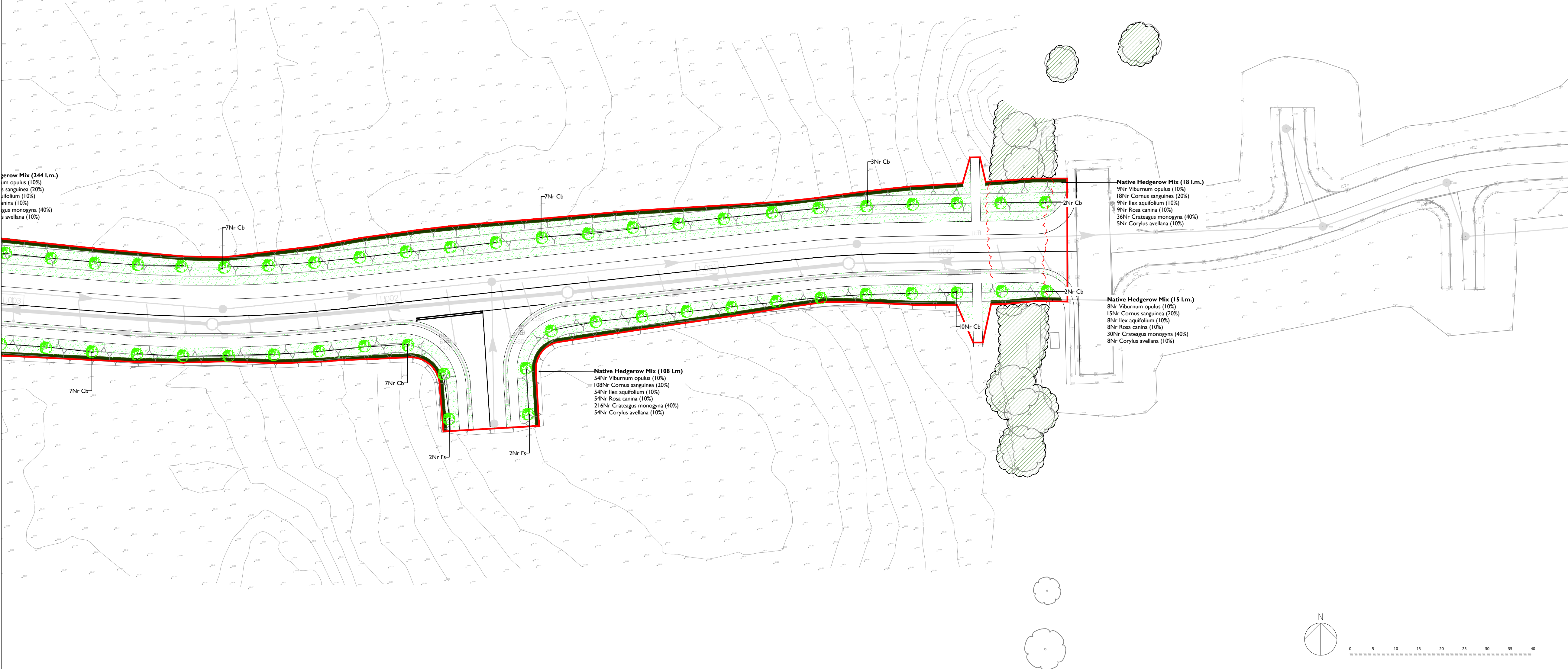
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Rev B - Adjustments to red line boundary
GBS 29/02/2024
Rev A - Adjustments to drainage and woodland description
GBS 10/05/2023



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Description
Masterplan incl. Planting (Sheet 3 of 3)

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Drawn
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5246

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01

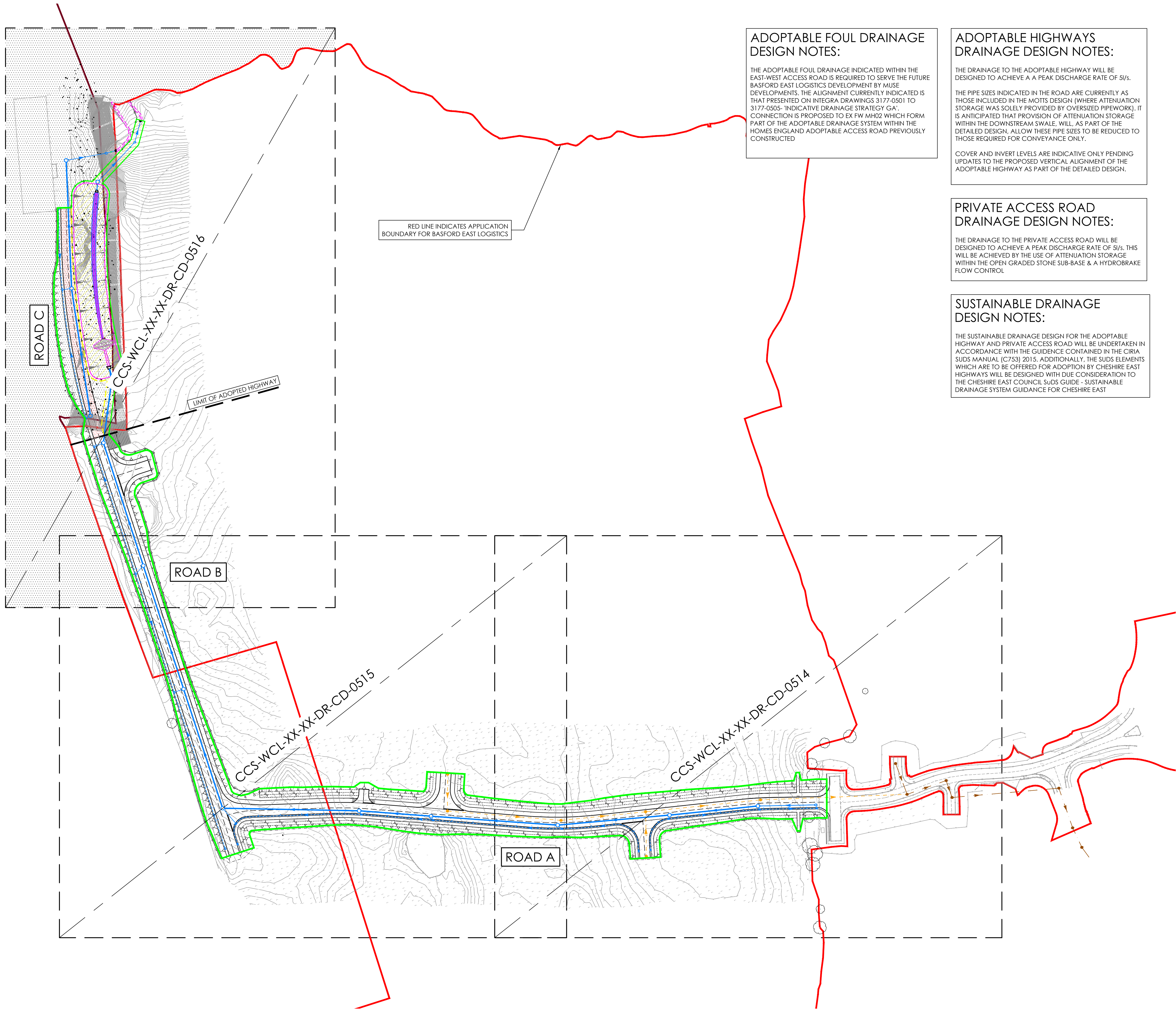
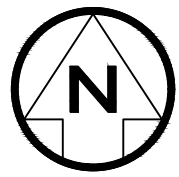
Date
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B

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ADOPTABLE FOUL DRAINAGE DESIGN NOTES:

THE ADOPTABLE FOUL DRAINAGE INDICATED WITHIN THE EAST-WEST ACCESS ROAD IS REQUIRED TO SERVE THE FUTURE BASFORD EAST LOGISTICS DEVELOPMENT BY MUSE DEVELOPMENTS. THE ALIGNMENT CURRENTLY INDICATED IS THAT PRESENTED ON INTEGRA DRAWINGS 3177-0501 TO 3177-0505- 'INDICATIVE DRAINAGE STRATEGY GA'. CONNECTION IS PROPOSED TO EX FW MH02 WHICH FORM PART OF THE ADOPTABLE DRAINAGE SYSTEM WITHIN THE HOMES ENGLAND ADOPTABLE ACCESS ROAD PREVIOUSLY CONSTRUCTED

ADOPTABLE HIGHWAYS DRAINAGE DESIGN NOTES:

THE DRAINAGE TO THE ADOPTABLE HIGHWAY WILL BE DESIGNED TO ACHIEVE A A PEAK DISCHARGE RATE OF 5l/s.

THE PIPE SIZES INDICATED IN THE ROAD ARE CURRENTLY AS THOSE INCLUDED IN THE MOTTS DESIGN (WHERE ATTENUATION STORAGE WAS SOLELY PROVIDED BY OVERSIZED PIPEWORK). IT IS ANTICIPATED THAT PROVISION OF ATTENUATION STORAGE WITHIN THE DOWNSTREAM SWALE, WILL, AS PART OF THE DETAILED DESIGN, ALLOW THESE PIPE SIZES TO BE REDUCED TO THOSE REQUIRED FOR CONVEYANCE ONLY.

COVER AND INVERT LEVELS ARE INDICATIVE ONLY PENDING UPDATES TO THE PROPOSED VERTICAL ALIGNMENT OF THE ADOPTABLE HIGHWAY AS PART OF THE DETAILED DESIGN.

PRIVATE ACCESS ROAD DRAINAGE DESIGN NOTES:

THE DRAINAGE TO THE PRIVATE ACCESS ROAD WILL BE DESIGNED TO ACHIEVE A PEAK DISCHARGE RATE OF 5l/s. THIS WILL BE ACHIEVED BY THE USE OF ATTENUATION STORAGE WITHIN THE OPEN GRADED STONE SUB-BASE & A HYDROBRAKE FLOW CONTROL

SUSTAINABLE DRAINAGE DESIGN NOTES:

THE SUSTAINABLE DRAINAGE DESIGN FOR THE ADOPTABLE HIGHWAY AND PRIVATE ACCESS ROAD WILL BE UNDERTAKEN IN ACCORDANCE WITH THE GUIDANCE CONTAINED IN THE CIRIA SUDS MANUAL (C753) 2015. ADDITIONALLY, THE SUDS ELEMENTS WHICH ARE TO BE OFFERED FOR ADOPTION BY CHESHIRE EAST HIGHWAYS WILL BE DESIGNED WITH DUE CONSIDERATION TO THE CHESHIRE EAST COUNCIL SUDS GUIDE - SUSTAINABLE DRAINAGE SYSTEM GUIDANCE FOR CHESHIRE EAST

NOTES:

- ALL DIMENSIONS ARE IN MILLIMETRES UNLESS NOTED OTHERWISE.
- ALL LEVELS ARE IN METRES (TO ORDNANCE DATUM) UNLESS NOTED OTHERWISE.
- DO NOT SCALE FROM THIS DRAWING.

LEGEND:

- PROPOSED HIGHWAY DRAINAGE
- PROPOSED HIGHWAY DRAINAGE MANHOLE
- PROPOSED ROAD GULLY
- PROPOSED FILTER DRAIN, FILTER DRAINS IN ADOPTABLE HIGHWAY UNDER FOOTWAY WITH POINT INFLOWS FROM GULLIES
- PROPOSED MUSE ADOPTABLE FW DRAINAGE (+ MANHOLE)
- EXISTING 'HOMES ENGLAND' ADOPTABLE FW SEWER (+ MANHOLE)
- PERMEABLE ASPHALT WITH OPEN STONE SUB-BASE
- EASEMENT FOR ADOPTABLE HIGHWAY DRAINAGE
- PLANNING APPLICATION BOUNDARY FOR BASFORD EAST LOGISTICS
- PLANNING APPLICATION BOUNDARY FOR ACCESS ROAD
- ARRIVA LEASE BOUNDARY
- NETWORK RAIL FREEHOLD OWNERSHIP
- LHA MAINTENANCE PARKING/ LAYDOWN AREA

P06	ET	KM	TB	14.03.24	MINOR UPDATES
P05	ET	KM	TB	08.02.24	OUTFALL ROUTES AMENDED
P04	AP	KM	TB	03.10.23	ARRIVA LEASE BOUNDARY AND MAINTENANCE PARKING SHOWN
P03	ET	KM	LG	04.04.23	ACCESS ROAD PLANNING BOUNDARY SHOWN
P02	ET	KM	LG	24.03.23	UPDATED TO REFLECT FEEDBACK FROM DESIGN TEAM
P01	ET	KM	LG	17.03.23	FIRST ISSUE

REV	DRN	CHK	APP	DATE	DESCRIPTION
1				14.03.24	MINOR UPDATES

Wilde
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Stockport, Cheshire,
SK1 3HS.
www.wildecivil.co.uk
Tel : 0161-474-7479
Fax : 0161-474-7492

Client

WORLD-CLASS INFRASTRUCTURE

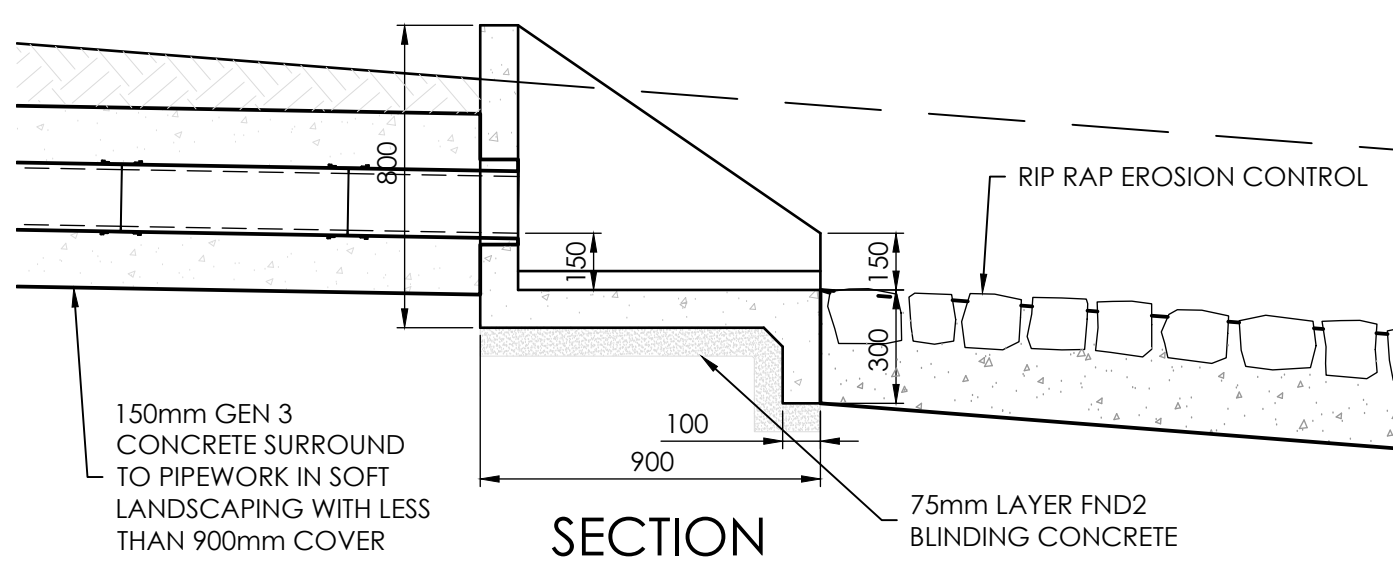
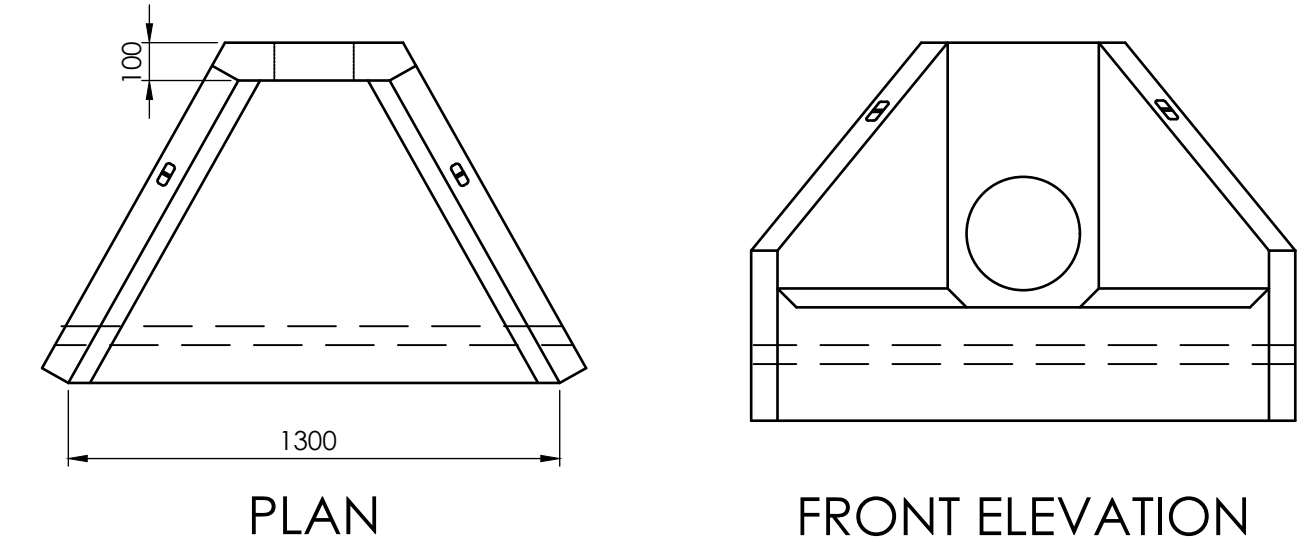
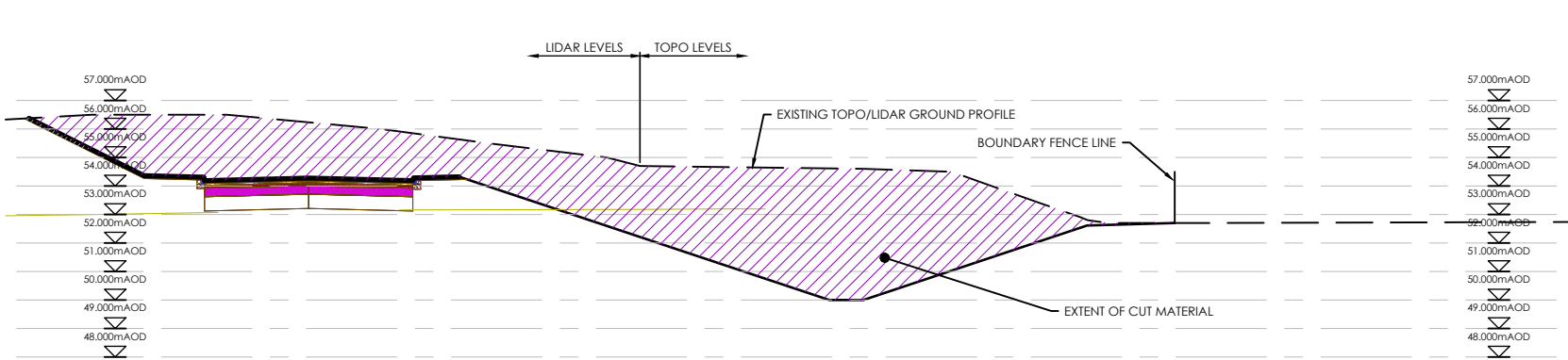
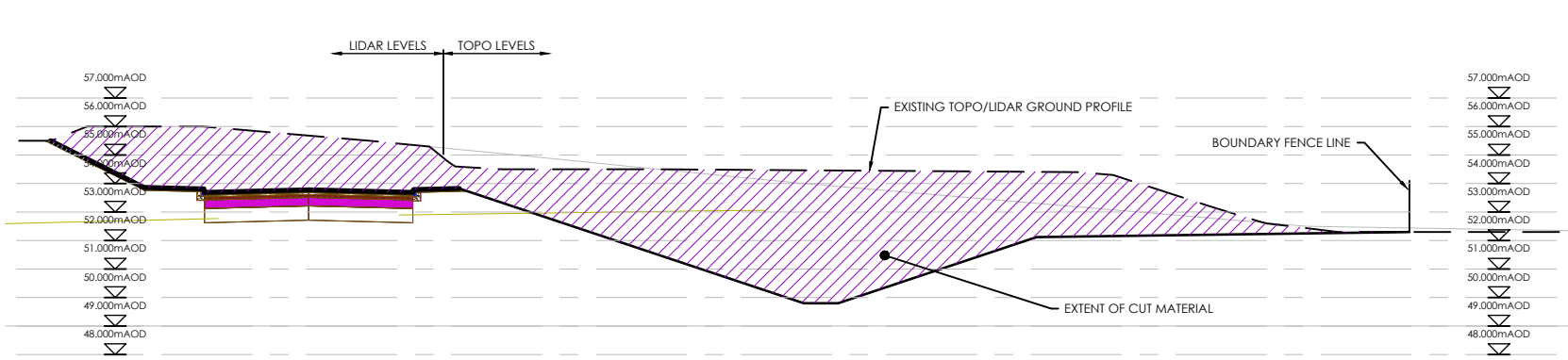
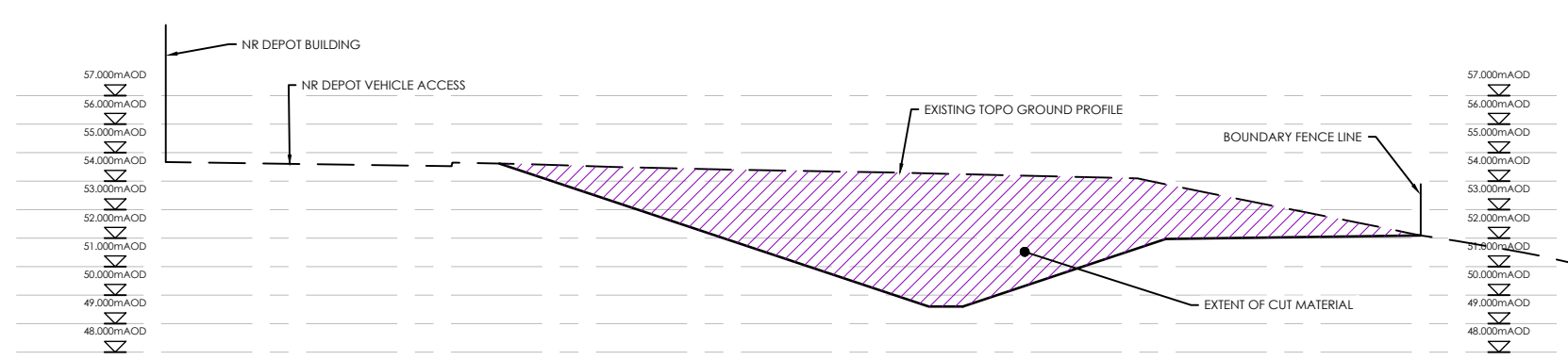
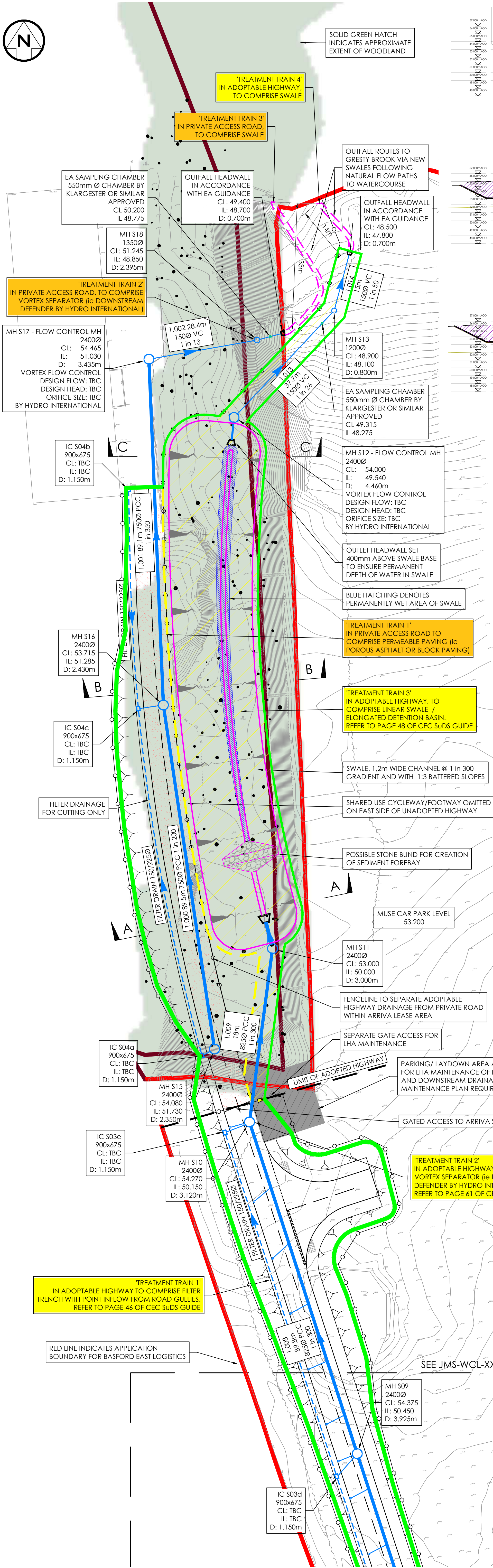
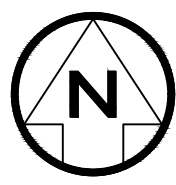
Project

ARRIVA TRAIN CARE ACCESS ROAD

Title

DRAINAGE STRATEGY OVERVIEW

Scale 1:1250	Drawn ET	Date MAR '23	Chkd KM	Approved LG
Drawing No. CCS-WCL-XX-XX-DR-CD-0513				Revision P06
Status INFORMATION				Sheet Size A1



NOTES:

- ALL DIMENSIONS ARE IN MILLIMETRES UNLESS NOTED OTHERWISE.
- ALL LEVELS ARE IN METRES (TO ORDNANCE DATUM) UNLESS NOTED OTHERWISE.
- DO NOT SCALE FROM THIS DRAWING.

LEGEND:

- PROPOSED HIGHWAY DRAINAGE
- PROPOSED HIGHWAY DRAINAGE MANHOLE
- PROPOSED ROAD GULLY
- PROPOSED FILTER DRAIN, FILTER DRAINS IN ADOPTABLE HIGHWAY UNDER FOOTWAY WITH POINT INFLOWS FROM GULLIES
- PROPOSED MUSE ADOPTABLE FW DRAINAGE (+ MANHOLE)
- EXISTING 'HOMES ENGLAND' ADOPTABLE FW SEWER (+ MANHOLE)
- PERMEABLE ASPHALT WITH OPEN STONE SUB-BASE
- EASEMENT FOR ADOPTABLE HIGHWAY DRAINAGE
- PLANNING APPLICATION BOUNDARY FOR BASFORD EAST LOGISTICS
- PLANNING APPLICATION BOUNDARY FOR ACCESS ROAD
- ARRIVA LEASE BOUNDARY
- NETWORK RAIL FREEHOLD OWNERSHIP
- LHA MAINTENANCE PARKING/ LAYDOWN AREA

ADOPTABLE HIGHWAYS DRAINAGE DESIGN NOTES:

THE DRAINAGE TO THE ADOPTABLE HIGHWAY WILL BE DESIGNED TO ACHIEVE A PEAK DISCHARGE RATE OF 5l/s.

THE PIPE SIZES INDICATED IN THE ROAD ARE CURRENTLY AS THOSE INCLUDED IN THE MOTTS DESIGN (WHERE ATTENUATION STORAGE WAS SOLELY PROVIDED BY OVERSIZED PIPEWORK). IT IS ANTICIPATED THAT PROVISION OF ATTENUATION STORAGE WITHIN THE DOWNSTREAM SWALE, WILL, AS PART OF THE DETAILED DESIGN, ALLOW THESE PIPE SIZES TO BE REDUCED TO THOSE REQUIRED FOR CONVEYANCE ONLY.

COVER AND INVERT LEVELS ARE INDICATIVE ONLY PENDING UPDATES TO THE PROPOSED VERTICAL ALIGNMENT OF THE ADOPTABLE HIGHWAY AS PART OF THE DETAILED DESIGN.

PRIVATE ACCESS ROAD DRAINAGE DESIGN NOTES:

THE DRAINAGE TO THE PRIVATE ACCESS ROAD WILL BE DESIGNED TO ACHIEVE A PEAK DISCHARGE RATE OF 5l/s. THIS WILL BE ACHIEVED BY THE USE OF ATTENUATION STORAGE WITHIN THE OPEN GRADED STONE SUB-BASE & A HYDROBRAKE FLOW CONTROL

SUSTAINABLE DRAINAGE DESIGN NOTES:

THE SUSTAINABLE DRAINAGE DESIGN FOR THE ADOPTABLE HIGHWAY AND PRIVATE ACCESS ROAD WILL BE UNDERTAKEN IN ACCORDANCE WITH THE GUIDANCE CONTAINED IN THE CIRIA SUDS MANUAL (C753) 2015. ADDITIONALLY, THE SUDS ELEMENTS WHICH ARE TO BE OFFERED FOR ADOPTION BY CHESHIRE EAST HIGHWAYS WILL BE DESIGNED WITH DUE CONSIDERATION TO THE CHESHIRE EAST COUNCIL SUDS GUIDE - SUSTAINABLE DRAINAGE SYSTEM GUIDANCE FOR CHESHIRE EAST

ADOPTABLE FOUL DRAINAGE DESIGN NOTES:

THE ADOPTABLE FOUL DRAINAGE INDICATED WITHIN THE EAST-WEST ACCESS ROAD IS REQUIRED TO SERVE THE FUTURE BASFORD EAST LOGISTICS DEVELOPMENT BY MUSE DEVELOPMENTS. THE ALIGNMENT CURRENTLY INDICATED IS THAT PRESENTED ON INTEGRA DRAWINGS 3177-0501 TO 3177-0505 - 'INDICATIVE DRAINAGE STRATEGY GA'. CONNECTION IS PROPOSED TO EX FW MH02 WHICH FORM PART OF THE ADOPTABLE DRAINAGE SYSTEM WITHIN THE HOMES ENGLAND ADOPTABLE ACCESS ROAD PREVIOUSLY CONSTRUCTED

WOODLAND DESIGNATION:

SEE ADAS LETTER REPORT DATED 15th JANUARY 2024 WHICH SUBSTANTIATES WOODLAND DESIGNATION AS NON-PRIORITY.

P07	ET	KM	TB	14.03.24	MINOR UPDATES
P06	ET	KM	TB	16.02.24	WOODLAND NOTES UPDATED
P05	ET	KM	TB	08.02.24	OUTFALL ROUTES AMENDED, SAMPLING CHAMBERS & HEADWALL DETAIL ADDED
P04	AP	KM	TB	03.10.23	ARRIVA LEASE BOUNDARY AND MAINTENANCE PARKING SHOWN
P03	ET	KM	LG	04.04.23	ACCESS ROAD PLANNING BOUNDARY SHOWN
P02	ET	KM	LG	24.03.23	UPDATED TO REFLECT FEEDBACK FROM DESIGN TEAM
P01	ET	KM	LG	17.03.23	FIRST ISSUE
REV	DRN	CHK	APP	DATE	DESCRIPTION



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Consulting Civil & Structural Engineers



Project
ARRIVA TRAIN CARE ACCESS ROAD

Title
DRAINAGE STRATEGY LAYOUT SHEET 3 OF 3

Scale 1:500	Drawn ET	Date MAR '22	Chkd KM	Approved LG
Drawing No. CCS-WCL-XX-XX-DR-CD-0516	Revision P07	Status PRELIMINARY	Sheet Size A1	

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Application No: 23/2945M

Location: Land Off, LEEK ROAD, BOSLEY

Proposal: Use of land as a touring caravan and camping site (74 pitches), amenity/reception building, new access & ancillary works

Applicant: Mr Lionel Cox, Healthy Happy Hub Ltd

Expiry Date: 28-Jun-2024

SUMMARY

The application site lies within the Open Countryside within the Peak Park Fringe Local Landscape Designation Area.

The proposed camping and caravan site would be an inappropriate scale in relation to the location and setting of the site, and no evidence has been submitted to demonstrate that there is an identified need for the proposal. The development would also have an adverse effect on the character of the surrounding landscape and the visual qualities of the countryside within which it is located. It has also not been demonstrated that a net gain for biodiversity would be achieved due to a lack of information. The proposal therefore conflicts with the development plan on these matters, which collectively carry substantial weight against the proposal.

No concerns are raised with regard to highway safety matters, trees, flood risk or drainage, contamination, loss of best or most versatile agricultural land or subject to conditions where appropriate. However, these are not considered to represent stand-alone 'benefits', but requirements of the application proposals to adhere to policy requirements and are considered to carry neutral weight in the planning balance.

Turning to the benefits of the proposal, the campsite would provide opportunities for visitors to access the countryside, which does carry some limited weight in favour of the proposal. There would also be some knock-on economic benefits realised in terms of job creation during the construction period and longer-term jobs on the site (stated to be 5 full time equivalent posts) and through business rates/taxes. The planning statement suggests that the development would support the local economy through the provision of tourist accommodation and support to the supply chain and the associated spending on local visitor attractions and shops/services. However, the construction period is unlikely to be particularly long given the nature of the development and the extent of construction activities required, therefore construction jobs would be very short-term. Furthermore, due to the lack of shops and services in the vicinity of the site, there would be very limited economic benefit to the area where the impacts of the development would be most evident. Accordingly, only limited weight is afforded to the economic benefits of the proposal.

The benefits of the proposal therefore do not outweigh the identified conflict with the development plan.

SUMMARY RECOMMENDATION

Refuse

REASON FOR REFERRAL

This application is referred to the Strategic Planning Board because the site area is over 4ha hectares and in line with the Council's Constitution it requires a Committee decision by the Strategic Planning Board.

DESCRIPTION OF SITE AND CONTEXT

The application site comprises over 4 hectares of open fields currently used for sheep grazing. The site is approximately 5km to the south of Macclesfield and 5km to the east of Congleton and is located within the open countryside.

The site is relatively level with a small pond within the centre of the site. The site is surrounded by agricultural land with a small residential estate and a number of residential park homes located to the south. Isolated dwellings and farmsteads lie to the north and west, with a commercial premises to the north east. Bosley reservoir is located approximately 200m to the south east, which is also a Local Wildlife Site. The A54 and A523 roads run along the from the north and south western boundaries on the site, Fold lane runs along the south east. The site is bordered by a post and rail fence, hedgerows and trees.

Bosley public rights of way FP1 and FP9 run around 250M to the east with Bosley FP 15 running along the Macclesfield Canal to around 900m to the west.

The site is located within flood zone 1 and generally within in an area at very low risk from surface water flooding, with bands of low risk within the site and several small areas within high-risk areas (topographic low spots within the site).

DESCRIPTION OF PROPOSAL

This application seeks full planning permission for the change of use of the land to a touring caravan and camping site. The site would comprise the siting of 74 pitches for touring caravans, motorhomes and tents arranged in clusters throughout the site. An amenity building would be located to the west, along with two permanently sited wardens caravans and parking area. A new vehicular and pedestrian access would be taken from Leek Road with an additional emergency exit on Buxton Road to the north and Fold Lane to the South. Pedestrian only access points are located to the south west and east with a circular route laid out within the site.

The proposed amenity building would be constructed from woodgrain Upvc, have a footprint of 13m by 6m, and a height of 3.5m to the ridge. Each of the units for warden accommodation would be 13m by 4m, with a ridge height of 4m, featuring a separate bedroom, kitchen, lounge, shower room and utility room. Measurements taken from the plans suggest that each of these meets the dimension requirements for a caravan, although the Design & Access Statement refers to the amenity building as the "only building".

The following plans and documents accompany the application;

- Planning Design and Access statement;
- Proposed Warden Accommodation
- Proposed Amenity Building

- Transport Statement
- Landscape and visual appraisal
- Flood Risk Assessment
- Ecological Report

Additional information was requested during the course of the application to address concerns raised by the planning officer and consultees, including updated ecological data and drainage details which has not been provided.

RELEVANT HISTORY

None

RELEVANT PLANNING POLICY

Cheshire East Local Plan Strategy 2010-2030 (CELPS)

MP1 Presumption in favour of sustainable development

PG1 Overall Development Strategy

PG6 Open Countryside

SD1 Sustainable Development in Cheshire East

SD2 Sustainable Development Principles

EG 2 Rural Economy

EG 4 Tourism

SC1 Leisure and Recreation

SC3 Health and Well-being

SE1 Design

SE3 Biodiversity and Geodiversity

SE4 The Landscape

SE5 Trees, Hedgerows and Woodland

SE6 Green Infrastructure

SE12 Pollution, Land contamination and land instability

SE13 Flood risk and water management

SE15 Peak District National Park Fringe

CO1 Sustainable Travel and Transport

Cheshire East Site Allocations and Development Policies Document (SADPD)

PG9 Settlement Boundaries

GEN1 Design principles

RUR 6 Outdoor sport, leisure and recreation outside of settlement boundaries

RUR 8 Visitor Accommodation outside of Settlement Boundaries

RUR 9 Caravan and Camping Sites

ENV 1 Ecological Network

ENV2 Ecological implementation

ENV 3 Landscape character

ENV5 Landscaping

ENV6 Trees, hedgerows and woodland implementation

ENV7 Climate Change

ENV12 Air quality

ENV14 Light pollution

ENV15 New development and existing uses
ENV16 Surface water management and flood risk
ENV17 Protecting water resources
HOU 12 Amenity
INF1 Cycleways, bridleways and footpaths
INF3 Highways safety and access
INF6 Protection of existing and proposed infrastructure
INF9 Utilities

National Planning Policy / Guidance

National Planning Policy Framework National Planning Policy Guidance

No made neighbourhood plan

CONSULTATIONS (External to Planning)

Canals and Rivers Trust – Recommend conditions regarding drainage, and an external lighting plan.

United Utilities – There are no known sewers in the vicinity of the site. Strongly recommend all developments include sustainable drainage systems to help manage surface water.

Strategic Highways (CEC) – No objections.

Environmental Protection (CEC) – Updated comments.

Amenity – Site license required in accordance with Caravan Sites and Control of Development Act 1960 and the Mobile Homes Act 2013. Informatives recommended regarding hours of construction and dust management plan.

Air Quality – Condition required regarding EV charging

Contaminated Land – No objection. Recommend that conditions for reporting unexpected contamination and informative for environmental protection act.

Lead Local Flood Authority – Holding objection. Requested drainage details for the amenity block and outfall details from the development.

Peak District National Park Planning Authority- Consideration of the adequacy of the scope of the LVIA in terms of the potential impact of the development on the setting of the National Park must be undertaken prior to any decision. If approved, a condition is required which prevents the installation of static caravan, chalets or lodges at the site.

Bosley Parish Council – The Parish council raises the following concerns:

- Visual impact may be significant from local viewpoint and wider views and from the Peak District National Park.
- Wardens block may impact negatively
- Further clarification on screening is sought
- Insufficient and poorly located amenity facilities (may increase demand for additional buildings in future)
- Lack of detail regarding reprofiling and access track construction
- No complete route around reservoir contrary to statement in the application
- Redaction is excessive in ecological report

- Loss of community orchard trees
- Inappropriate lighting may affect neighbours and visual amenity
- No demand/evidenced need for facility
- Proposals bring no value to the community
- No energy efficiency measures included

The PC also make the following point:

- Proposal would support tourism and bring visitors to the local pub and generate limited employment
- Proposals would need to be adequately licensed.

REPRESENTATIONS

A petition has been received containing 228 signatures of people who do not agree with the proposed use of land, asking that planning permission is not granted and noting that it will “*spoil the countryside in the village of Bosley*”.

105 letters of representation have been received from neighbours, CPRE, and other interested parties objecting to the proposal on the following grounds:

Principle

- Lack of infrastructure
- Lack of amenities in the surrounding area
- Scale of development so close to existing dwellings
- Scale of disproportionate to the population of Bosley
- Impact on quality of life for residents
- Permanent change to character of area
- There is already a small campsite serving the village
- Existing facilities (i.e. playground are only big enough for small number of residents)
- Unsustainable location – 4mile round trip to nearest shop
- Only 1 pub in the village
- Transient campsite would contribute little to village
- Proposals could equate to around 222 visitors plus staff which is over half the population of Bosley (population was 400 people in 2011)
- Scale not appropriate and no identified need
- No benefit to community
- Not essential as village already has a campsite
- Inappropriate location
- No tourism need or economic impact assessment submitted to justify development
- Proposals would dominate village
- Contrary to CE local plan policy
- Inappropriate overdevelopment
- No demonstration that alternative sites have been considered
- There are 178 campsites within 20miles of Bosley, why is another needed?
- Development would cause undue stress to local residents
- Rural community is disappearing at a fast pace and this will not help

Visual Amenity

- Detrimental to character of area which is on fringe of Peak District
- Loss of beautiful views

- Change to landscape
- Proposals would be a blot on the landscape
- Alteration to appearance of area from agricultural unimproved wet meadowland to large scale engineered development
- Development would urbanise and undeveloped part of the landscape
- It would be visually prominent from north, south and east
- Field is currently wild and natural looking
- Ugly white boxes would spoil the Peak fringe location
- Visually intrusive and detrimental to character of the area
- Reduction in openness
- Reduction in attractiveness of the village
- Cramped layout, not enough room for facilities for campers
- Site is a gateway to the village and will not retain Bosleys character

Highways

- Lack of footpaths on the area
- Lack of passing places in surrounding lanes
- No footpath around reservoir, informal path used is unsafe
- No parking in the village
- Entrance to site too close to 4 way junction
- No footpaths on Buxton road for pedestrians
- 74 Pitches = 74+ vehicles
- Access is not wide enough for 2no twin axle caravans to pass
- Bus service in village is very limited and do not run in evenings
- Current footpaths are poorly maintained
- Increase in footfall would require footpath improvements
- Substantial increase in vehicular movements
- Introducing 200+cars would be dangerous and irresponsible
- All footpaths are accessed by busy A road and do not have safe access
- Bus runs 2hourly and therefore car reliant development
- Dangerous crossroads subject to accidents already
- Increased traffic generation
- Access point not suitable

Ecology

- Impact on flora and fauna in immediate area
- Impact on owls and birds
- Diverse fauna would loss out from loss of habitat
- We need to preserve the environment, wildlife and rural appeal of countryside
- Frogs, Toads, and common newts, Barn Owls, Sparrow Hawks, Woodpecker, and a range of wild fowl all present in field
- Lighting would affect bats that's feed in evening
- Devastating impact on biodiversity
- Threat to poaching and wildlife
- Insects found on site are susceptible to pollution
- Large parts of ecology report are redacted and therefore transparency is questioned

Flooding

- Site floods regularly [Many photos of flood events have been supplied by residents]
- Field acts as a natural soakaway, highway drains are all blocked

- Drainage system already at capacity
- Sewage system at eastern end of site could release effluent into reservoir

Environmental

- Noise pollution
- Light pollution
- Increased litter
- Profound effect on residents of Bosley
- Potential for anti-social behaviour
- Transient and temporary visitors may not treat area as local would
- The pond on site is as a result of artificial excavations
- No planned method of water disposal
- Increased in dog fouling
- Site could be open 24hrs and would be a nuisance
- Allowing addition screening is good but will cause hazards for road users
- Waste tanks of existing developments smell and overflow into the streams and assume this will happen for this site
- LVIA does not take into account impact locally or on PDNP
- Proposals would affect water supply for local residents
- Waste from site would attract vermin
- Insufficient evidence to demonstrate proposals would not result in unacceptable landscape impacts
- Vehicle pollution would not help climate change

Amenity

- Unacceptable loss of privacy
- Impacts of cooking/noise and general activity from visitors would be detrimental to countryside and residents
- Too close to existing residents
- Dominating effect of temporary and permanent reception buildings
- Unsubstantiated comments regarding prevailing wind reducing noise on site
- Overbearing effect of new buildings
- Neighbouring fences are all low this would be a massive invasion of privacy
- Toilets and bins next to neighbouring business will cause smells and nuisance

Other

- Property depreciation
- Hedges and fences are not maintained by landowner
- Scrap tyres litter the site
- Not enough clearance for the electrical cables overhead
- Anomalies in the application
- Greater risk of crime and vandalism
- Potential contamination from former petrol station to north west
- What provision has been made for power to the site
- The land is vital for food production
- Reservoir suffered in lockdown from increase in visitors
- Is this a viable business? Who would want to camp on a busy main road?
- Once tarmac and concrete go in a housing application would follow.
- Increased footfall around reservoir would have adverse effect on angling

- The small set of caravans in Bosley are already occupied all year contrary to PP
- Security risk to neighbouring business
- No planting plan submitted
- Greed driven application
- Present owner has neglected the field
- Potential for anti-social behaviour

1 representation of support summarised below;

- Beneficial to the area bring revenue to local businesses.

OFFICER APPRAISAL

Principle of development

The site is located on the edge of the village of Bosley; a small and scattered village consisting of large open areas of countryside and pockets of residential dwellings, and some commercial development, located beyond established settlement boundaries within the open countryside. There are few facilities within the village, but these include a public house, a church and a primary school.

CELPS policy PG6 states that within the Open Countryside only development that is essential for the purposes of agriculture, forestry, outdoor recreation, public infrastructure, essential works undertaken by public service authorities or statutory undertakers, or for other uses appropriate to a rural area will be permitted.

CELPS policy EG2 provides support for rural based tourist attractions and visitor facilities, where the development meets certain criteria. These include a requirement that the development:

- Meets sustainable development objectives as set out in policies MP 1, SD 1 and SD 2
- Is consistent in scale with its location and does not adversely affect nearby buildings and the surrounding area or detract from residential amenity;
- Is well sited and designed in order to conserve and where possible enhance the character and quality of the landscape and built form; and
- Does not conflict with Policies PG 3, PG 4, PG 6, PG 7, SE 3, SE 4, SE 5, SE 6, and SE 7 of the Local Plan Strategy.

CELPS policy EG4 seeks to protect and enhance Cheshire East and its unique features which attract tourists. One of the strategies for this is the enhancement and expansion of existing visitor attractions and tourist accommodation, and the provision of new visitor and tourism facilities, in sustainable and appropriate locations. Relevant to this application, the policy goes on to state that proposals outside of Principal Towns and Key Service Centres will be supported where:

i. Either:

- a. They are located within a Local Service Centre; or
- b. They are located within an existing or replacement building; or
- c. There is evidence that the facilities are required in conjunction with a particular countryside attraction;

ii. And:

- a. The scale, design and use of the proposal is compatible with its wider landscape or townscape setting and would not detract from the character or appearance of the area; and
- b. It would not be detrimental to the amenities of residential areas; and
- c. The proposals are served by adequate access and infrastructure; and
- d. The site has access to local services and employment

Similarly, SADPD policies RUR6, RUR 8 and RUR9 advise that certain types of visitor accommodation may be appropriate to a rural area where their scale is appropriate to the location and setting; where there is an identified need for the accommodation, which cannot be met in nearby settlements because the type of accommodation proposed is intrinsically linked with the countryside; and provided proposals do not unacceptably affect the amenity and character of the surrounding area (including visual impacts, noise, odour, design and appearance).

The development scheme proposes 74 pitches and the erection (permanent positioning) of 3 buildings / caravans (amenity building and 2 warden lodges). The village is a modest size with a population of approximately 450 people (2021 census). A small campsite is located within the village (adjacent to the Harrington Arms) and a permanent caravan park located to the north (Stoneyfold caravan park).

The site is not located within a Local Service Centre, is not located within an existing or replacement building, and no evidence that the facilities are required in conjunction with a particular countryside attraction has been submitted. The proposal is therefore not supported by policy EG4.

Policy RUR 9 relates specifically to caravan and camping sites. This policy explains that where their scale is appropriate to the location and setting, sites for touring caravans and camping are considered to be uses appropriate to a rural area, provided it can be demonstrated that a countryside location is necessary for the proposal.

Whilst the supporting planning statement makes brief reference to there being “*several caravan and camping sites beyond Cheshire East’s boundary in the Peal District, there is very little provision for touring caravans and camping within a 5 miles radius of the application site within Cheshire East*”, this does not demonstrate a countryside location is necessary in this case. It has also not been demonstrated that the scale of the proposed development is appropriate to the location and setting of the site. Bosley falls in the very lowest tier of Cheshire East’s settlement hierarchy outlined in policy PG2 of the CELPS. Falling within the “Other Settlements and Rural Areas” tier where proportionate development at a scale commensurate with the function and character of the settlement and confined to locations well related to the existing built-up extent of the settlement is permitted.

The site is located at the northern end of Bosley Village, adjacent to Bosley crossroads. The majority of the existing development in Bosley lies adjacent to the A523 Leek Road in a loose linear form. The application site is over 4ha in area and extends back from Leek Road by over 400m. The proposed 74 camping pitches are scattered across the full extent of the site. The scale of the 74-pitch site for caravans, camper vans and tents, and associated levels of activity, is considered to be disproportionate to the existing development within Bosley village, which has a population of approximately 450 people. The pattern of development within Bosley is such that there are small pockets of development separated by open spaces along the A523

for the full length of the village. None of these pockets of development are of a comparable scale to the application site. The facilities in Bosley are very limited and the site is some distance from shops and most services. The nearest shops are located within Macclesfield, some 6km to the north of the site. Most trips to and from the site will be reliant on the private car. When occupied the capacity the site has the potential to introduce large numbers of visitors that would overwhelm this very modest village. A proposal of this size would not relate well and would be completely out of scale and character with the local area.

Overall, the scheme would not be of a suitable scale appropriate to this location and setting and no evidence is submitted that demonstrate a countryside location is necessary for the proposal. The proposals are not considered to comply with the principal requirements of CELPS policies PG6, EG2 and EG4 or SADPD policies RUR 6, RUR 8 and RUR 9 in this regard.

Character and Appearance

Paragraph 180(b) of the NPPF states that planning decisions should recognise the intrinsic character and beauty of the countryside.

Paragraph 135 of the NPPF seeks to ensure that, amongst other things, developments function well and add to the overall quality of the area, be visually attractive as a result of good architecture, layout and landscaping; are sympathetic to local character and history, while not preventing or discouraging appropriate innovation or change; establish or maintain a strong sense of place, and create attractive and distinctive places to live, work and visit.

Policy SE1 of the CELPS sets out the design criteria for new development and states that development proposals should make a positive contribution to their surroundings. It seeks to ensure design solutions achieve a sense of place by protecting and enhancing the quality, distinctiveness and character of settlements. It should also respect the pattern, character and form of the surroundings. Policy SD2 of the CELPS further details the design matters that should be considered, including height, scale, form and grouping of development, choice of materials, external design features, massing of development and the balance between built form and green/public spaces.

SADPD Policy GEN 1 requires proposals to create high quality development reflecting local character and design and creating a sense of identity and legibility by using landmarks and incorporating key views into, within and out of new development and reflecting local character.

Policy SE4 of the CELPS seeks to conserve the landscape character and quality and where possible, enhance and effectively manage the historic, natural and man-made landscape features that contribute to local distinctiveness of both rural and urban landscapes. Policy SD2 advises that development should respect and, where possible, enhance the landscape character of the area.

The site lies within the countryside and is within a rural location. It is a relatively flat site, with higher ground beyond to the west towards the Peak District national Park and north towards Sutton. The flat setting of the site and limited screening to the A54 (along the northern boundary of the site) means that the site is prominent in short distance views from this highway. More limited views are also available from Fold Lane to the south and the A523 to the west. In addition, as noted within the LVIA and the consultation responses more distant views of the site are possible from the surrounding higher ground, notably from PRow Bosley FP01.

Only details of the external appearance of the amenity building have been provided (wood grain cladding and graphite pantile roof sheets). Whilst it is acknowledged that it is not unusual to see caravans in the countryside, caravans and motorhomes visiting the site will often have a light colouring, which would stand out and appear stark and incongruous in their surroundings. The proposals would introduce caravans (and/or buildings), parked cars and hard surfaces into an agricultural field otherwise free from built development. The visual impact would be exacerbated due to the scale of the proposals and their spread across the whole 4ha. site. The proposed layout is large and yet scattered, and it is a very open site with groupings of units and buildings. There is no explanation provided to justify this arrangement. The distance from the amenity building to the west of the site to the tent pitches to the very east is over 350m with the pitches positioned along this length, and visible from the above-mentioned visual receptors. The addition of lighting (albeit low-level) across a site of this scale would intensify this visual impact. The proposed pitches would be gravelled and would be served by gravelled access roads with two designated parking areas shown to the east and west, however it is assumed most vehicles would be parked alongside their caravan or tent. 74 pitches are proposed, with large areas of the site not marked out with pitches, although the capacity to accommodate more pitches clearly exists. The boundary hedgerow and trees are shown to remain, however there are no landscaping details to accompany the application.

The proposals would fundamentally alter the existing open and rural character of the site introducing urbanising features (access roads, vehicles, levels of activity etc.). This is at odds with the countryside location and would appear obtrusive and detrimental to the open character of the surrounding countryside.

It is proposed to maintain hedgerows at a greater height than at present and the planning statement advises that a woodland buffer would be established to the south of the site (to the rear of the existing residential properties), which is now shown on a plan. However, a tree buffer this would take many years to establish to a point where it is an effective screen. Furthermore, in order to be an appropriate landscape scheme for this area, this would need to comprise native deciduous trees which would offer sparse leaf coverage and screening during winter months.

The site lies within the Peak Fringe Local Landscape Designation Area (LLDA). This recognises that the area is an extension of many of the special qualities associated with the nationally protected Peak Park landscapes, such as; The naturally varied undulating landform and buildings of local materials which add a strong sense of rural place and pastoral character derived from the distinctive character of hamlets and villages. The area contributes a sense of escapism and remoteness despite the proximity of Congleton, Macclesfield and Bollington and this landscape acts as a buffer between urban areas and the National Park. CELPS Policy SE15 seeks to protect the setting of the Peak District National Park. Development will be considered on its individual merits having particular regard to the type, scale and location taking account of the Peak District National Park Landscape guidelines and characteristics of the South West Peak and the adjoining areas of the Cheshire Plain.

The site lies within landscape character type (LCT) 12: Upland foot slopes and landscape area (LCA)12C: Langley as identified in the Cheshire East Landscape Character Assessment.

The site currently comprises an open agricultural field. The farmland is attractive and characteristic of the Peak Fringe Area with panoramic views within the site out towards the south, north and east. An agricultural merchant and turf care supplier is located to the north

west corner of the site but sits relatively low in the landscape and is surrounded by a tree and hedge boundary.

Policy ENV3 of the SADPD outlines that development proposals should respect the qualities, features and characteristics that contribute to the distinctiveness of the local area, as described in the Cheshire East Landscape Character Assessment (2018) taking into account any cumulative effects alongside any existing, planned or committed development. Policy ENV5 of the SADPD sets out what should be included in landscaping plans.

As outlined above, SADPD policies RUR 6, RUR8 and RUR9 provide support for recreational visitor accommodation proposals that do not unacceptably affect the amenity and character of the surrounding area and subject to appropriate landscaping and screening being provided.

A landscape and visual assessment has been submitted providing an assessment of potential visual impact. The report considers the sensitivity of the landscape to be medium high. Whilst the assessment concludes that the site is able to accommodate the proposed tourist development without causing undue harm to the landscape character and visual amenity of the site and surrounding countryside, local landscape designation and the footpath network, the report advises that adverse effects identified would be mitigated by the potential planting scheme described within the landscape strategy. However, no detailed landscape proposals have been submitted.

The Peak District National Park Planning Authority (PDNPA) also note that the LVIA does not make reference to the PDNPA's Landscape Strategy. They advise that the application site is adjacent to an area which forms part of Open Moors Landscape Character type, identified as one of the 8 types within the South West Peak Character Area, which is not considered in the LVIA.

Whilst existing hedgerow and trees would provide a degree of mitigation for potential adverse effects at the operational phase. The Council's landscape officer advises that the existing boundary planting is not adequate to screen the proposals and would therefore have a harmful effect on the openness of the countryside, a detrimental visual effect on the 'Peak Fringe' and other local character areas. This site would be visible from higher ground at a distance, with the visual effects this will cause, with mobile caravans of various sizes and colours being very visible in the landscape, which will be especially noticeable at peak times such as during the summer.

The setting of the Church, School, residential properties and general built environment relies on gaps and breaks in development to soften the settlements edges. This proposal will harden such an existing gap within the settlement matrix and thus affect the local existing character area dramatically with negative consequences.

The Council's landscape officer objects to the proposal and considers the proposals to be too large, poorly designed regarding landscape and to have visual impacts which are unacceptable for the size, scale and positioning of the units and the red line.

Overall, the proposals would result in a dramatic and negative change to the character of this area by virtue of its inappropriate scale in relation to the location and setting of the site. The proposals would have an adverse effect on the character of the surrounding landscape and the visual qualities of the Countryside within which it is located and would be contrary to policies

SE1, SD2 and SE15 of the Cheshire East Local Plan Strategy, Policies GEN1, RUR 6, RUR 8 and RUR 9 of the Site Allocations Development Plan Document.

Living conditions

CELPS policy SE1 seeks to ensure appropriate levels of privacy for new and existing residential properties. Policy SD 2 also expects all development to contribute positively to an area's character and identity, creating or reinforcing local distinctiveness in terms of its relationship to neighbouring properties. SADPD policy HOU 12 seeks to ensure development does not cause unacceptable harm to the amenities of adjoining or nearby occupiers of residential properties, sensitive uses, or future occupiers of the proposed development due to:

1. loss of privacy;
2. loss of sunlight and daylight;
3. the overbearing and dominating effect of new buildings;
4. environmental disturbance or pollution; or
5. traffic generation, access and parking.

SADPD Policy HOU 13 (table 8.2) and the Cheshire East Design Guide set out the standards for space between buildings and the requirement to include an appropriate quantity and quality of outdoor private amenity space, having regard to the type and size of the proposed development. This is required to maintain an adequate standard of privacy and amenity between residential properties and provide appropriate amenity space for future occupants.

The closest neighbouring residential properties to the application site are the around 25m the south, and around 30m to the north. The planning statement advises a 20m deep landscape buffer would be planted in the south west section of the site.

Given the distance between the pitches and residential properties it is not considered that the development would result in any unacceptable neighbouring impacts in terms of loss of privacy, loss of light or overbearing impacts.

Residents are concerned about the potential for increased noise and disturbance and general activity from the site. The use will generate additional movements to and from the site from guests and people servicing the site and there will be a change to the noise impacts currently experienced by residents at the site. However, this is likely to be of a level comparable to a residential use and the site is close the main road where a level of traffic noise and movement is already experienced. The extant agricultural use could also generate a certain level of comings and goings from farm related traffic. There are wardens on site who could manage noise and guests at the site. Additionally, a site operation plan can be secured via condition to manage the site further. Consequently, no significant noise issues are raised.

It is therefore considered that the proposals comply with the principles of CELPS policy SE1, SADPD policies HOU 12 and 13 and advice within the Cheshire East Design Guide in this regard.

Highways/Accessibility

CELPS Policy CO 1 deals with sustainable travel and transport. It supports a shift from car travel to public transport and seeks to guide development to sustainable and accessible locations.

SADPD policy INF3 requires that amongst other things, proposals provide safe access to and from the site for all highway users and incorporate safe internal movement in the site to meet the requirements of servicing and emergency vehicles. Development traffic should be satisfactorily assimilated into the operation of the existing highway network so that it would not have an unacceptable impact on highway safety, incorporating measures to assist access to, from and within the site by pedestrians, cyclists and public transport users and meets the needs of people with disabilities.

Residents are concerned about the highway impacts of the development with regards to traffic generation, highway safety and adequacy of proposed access points and lack of footways in the surrounding immediate area. However, the highway officer has not raised any objections to the proposals.

The single main access to the site is a priority junction with the A523, it has 6m radii and is 5.5m at the bellmouth which reduces to 4.8m internally within the site. Visibility splays of 2.4m x 120m were proposed in both directions at the access. Following an assessment of these details the applicant was informed that due to the boundary landscaping and the bend on the A523, the visibility available in the leading direction would be compromised by the vegetation. The applicant has subsequently undertaken a speed survey on the vehicle approach speeds in the leading direction, the results of this survey indicate that the 85%ile speed is 36mph thereby leading to a reduced visibility splay of 60m which can be achieved at the proposed access point. This is considered acceptable by the highway officer.

Internally, the access track is circular and would likely be one way or the occurrence of meeting opposing vehicles would be relatively low. In any event the internal roads would be private and a matter for the applicant to manage.

There is a bin store within the site close to the amenity building and swept paths have been provided to show that refuse vehicles can access the site and can turn and exit the site.

Although the site is linked to the external pedestrian routes and there is a limited bus service on the A523 close to the site, the vast majority of trips will be vehicle based in this rural location. However, the level of traffic generation to and from the site is predicted to be low and not coincide with peak hours trips on the road network as they will predominantly be leisure trips.

The highway officer considers the proposed leisure use of the site would not result in any traffic/capacity problems on the local road network and the access is acceptable subject to conditions regarding visibility space and access laid out per approved plans prior to occupation.

The proposals therefore comply with CELPS Appendix C: Parking Standards and SADPD policy INF 3, and Policy CO1 of the CELPS.

Trees

CELPS Policy SE5 seeks to ensure the sustainable management of trees, woodland and hedgerows including provision of new planting to provide local distinctiveness within the landscape, enable climate adaptation resilience, and support biodiversity. Furthermore, the planting and sustainable growth of large trees within new development as part of a structured landscape scheme is encouraged in order to retain and improve tree canopy cover within the borough as a whole. Similarly, SADPD policy ENV 6 requires proposals to retain and protect trees, woodland and hedgerows. Proposals should include measures to secure the long-term maintenance of newly planted trees.

The application site is located within open countryside and benefits from established and high amenity field boundary trees and hedgerows. The site is not within a Conservation Area and no Tree Preservation Orders are present on the site.

The application is not supported by any arboricultural information although tree positions are identifiable on supporting plans. A linear group of trees to the northeast side of the proposed main access and outside the site boundary are afforded protection by the Macclesfield Borough Council (Bosley – Former Crossroads Filling Station) Tree Preservation Order 1990.

Whilst it appears that the boundary trees may be retained, the creation of the main vehicular access to the site would pass between retained trees and result in the loss of a section of agricultural hedgerow which may be subject to the Hedgerow Regulations 1997. Additionally, the proposed Suds concept design within the FRA suggests that a swale could be incoming to the site from Leek Road.

In the absence of an arboricultural survey which confirms tree and hedgerow positions and root protection areas it is unclear what the true impacts of the proposal would be once these elements of the layout are confirmed. However, the Council's tree officer considers that the proposals could include the retention of important trees, and that the plans suggest that they are retained. In the event that the application is approved, conditions for tree protection and submission of an arboricultural method statement which considers any final SUDS scheme and drainage layout to ensure the retention of all existing established boundary trees, is recommended.

Where proposed development is likely to result in the loss of existing agricultural hedgerows which are more than 30 years old, a Hedgerow Removal Notice would be normally required under the Hedgerow Regulations 1997. However, there are some circumstances where hedge removal can be exempt from the Regulations, for example,

- To make a new opening in substitution for an existing one which gives access to land (in which case the existing access must be planted up with hedge plants within 8 months),
- or
- To obtain access to land where another means of access is not available or is only available at disproportionate cost.

The tree officer considers that the hedge in question may meet one of the historic criteria in the Regulations. Nevertheless, if the applicant can demonstrate that an exemption would be applicable, a full assessment may not be required but the amount of hedgerow to be removed in its entirety to accommodate the access should be indicated on the proposed plans.

Subject to conditions, the proposal is considered to comply with policies SE5 and ENV6 of the local plan.

Flooding and Drainage

Policy SE13 of the CELPS states that developments must integrate measures for sustainable water management to reduce flood risk, avoid an adverse impact on water quality and quantity within the borough and provide opportunities to enhance biodiversity, health and recreation.

Representations have raised concerns over flood risk at the site and have supplied photographic evidence of recent flood events on the site and in the locality.

The site is located within Flood Zone 1, indicating that the site is not at risk from fluvial or tidal sources according to the Flood Map for Planning. Although the site is generally identified as being a low risk for surface water flooding, there are areas of high risk within the site. The Flood Risk Assessment (FRA) accompanying the application considers reservoir breaches from Bosley reservoir which would occur south and flow towards the River Dane. The Canal and River Trust agree with the findings.

The Local Lead Flood Authority have placed a holding objection on the application seeking further details regarding drainage. Whilst this was raised with the applicant, no further detail has been submitted.

The FRA proposes that development is directed to areas with no surface water flood risk within the site and that 3 swales are incorporated into key flow paths in the site with run off directed to the existing pond on site and infiltration managed on site. It is also proposed that the access road and pitches for caravans have a permeable gravel substrate. The report concludes that the proposals are not expected to increase flood risk elsewhere although makes recommendations for further investigations of drainage features on site.

It is considered that conditions could appropriately deal with drainage design and management at the site and therefore subject to these conditions the proposals would accord with policy SE13 of the CELPS and the NPPF in this regard.

Contamination

CELPS policy SE12 seeks to ensure that all development is located and designed so as not to result in a harmful or cumulative impact upon air quality, surface water and groundwater, noise, smell, dust, vibration, soil contamination, light pollution or any other pollution which would unacceptably affect the natural and built environment, or detrimentally affect amenity or cause harm. In most cases, development will only be deemed acceptable where it can be demonstrated that any contamination or land instability issues can be appropriately mitigated against and remediated, if necessary.

The application is for a proposed use that would be vulnerable to the presence of contamination that may exist due to the former agricultural use of the site. The Council's Environmental Health officer has advised that a condition regarding the reporting of unexpected contamination would be sufficient to address any potential issues and informatics can advise the applicant of their obligations under the Environmental Protection Act 1990, Caravan Sites and Control of Development Act 1960 and the Mobile Homes Act 2013, dust and hours of construction.

A condition has also been recommended by Environmental Health regarding the provision of Electric Vehicle Infrastructure in the interests of protecting air quality.

It is therefore considered that subject to conditions the proposed development would comply with Policy SE12 of CELPS and the NPPF in this regard.

Agricultural Land Quality

Policy SD1 of the CELPS states that development should, wherever possible (and amongst other matters), protect the best and most versatile agricultural land. Policy SD2 of the CELPS states that all development will be expected to avoid the permanent loss of areas of agricultural land quality 1, 2 or 3a, unless the strategic need overrides these issues.

Paragraph 180(b) of the NPPF states that planning decisions should recognise the intrinsic character and beauty of the countryside, and the wider benefits from natural capital and ecosystem services – including the economic and other benefits of the best and most versatile agricultural land. Agricultural land falling within classes 1-3a are classed as ‘Best and Most Versatile’ BMV.

According to the 2010 Natural England Land Classification Map for the North West Region, the site falls within land which is Grade 4 ‘poor’ quality.

According to a more up-to-date (2017) map produced by Natural England, which considers the likelihood of parcels of land being Best and Most Versatile, the map shows that the site as being of moderate likelihood of BMV.

In light of the above it is not considered that the proposal would result in a loss of BMV agricultural land.

Ecology and Biodiversity

Section 15 of the NPPF considers the conservation and enhancement of the natural environment. Local Plan Policy SE 3(5) requires all developments to aim to positively contribute to the conservation of biodiversity. This is echoed within SADPD policy ENV 2.

Interested parties have highlighted concerns about the impacts on wildlife and biodiversity as a result of the proposals. The ecological assessment accompanying the application advises that the site is 5m from Bosley Reservoir Local Wildlife Site (LWS), across Fold Lane, and anticipates negligible impact to the LWS.

Biodiversity Net Gain

In accordance with Local Plan policy SE3(5) all development proposals must seek to lead to an overall enhancement for biodiversity. This planning application provides an opportunity to incorporate features to increase the biodiversity value of the final development in accordance with Local Plan Policy SE 3. To achieve net gain for biodiversity it should be ensured that any habitats that are higher value (such as ponds and woodland, more species rich grassland etc) are retained and enhanced as part of the development proposals. In order to assess the overall loss/gains of biodiversity, an assessment undertaken in accordance with the Defra Biodiversity ‘Metric’ version 4 should be undertaken. This has been requested from the applicant, but no further details have been submitted.

Ecological Network Map

The application site falls within the CEC ecological network which forms part of the SADPD. Policy ENV1 therefore applies to this application. The Council's ecologist considers that an ecological enhancement scheme could be required by condition to satisfy this requirement. However, as noted above, the baseline ecological position still needs to be established (as part of the Defra Biodiversity metric assessment) so the full impacts of the development can be identified, and appropriate ecological enhancement strategy can be secured.

Wildlife sensitive lighting

Due to the potential impacts of the proposed lighting scheme upon bat roost features, bat commuting and foraging habitat (boundary hedgerows, trees, watercourses etc.), if the application is approved a condition is recommended requiring details of the proposed lighting to be submitted and approved,

Breeding Birds

A number of bird species were recorded as nesting on site. Planning conditions can safeguard nesting birds and to ensure the incorporation of measures to provide replacement nesting opportunities could be included on any approval.

Ecology and Biodiversity Conclusions

In the absence of a biodiversity metric calculation, it is not possible to identify the full biodiversity impacts of the proposal or determine whether the development would provide for a net gain in biodiversity in line with the expectations of national policy and the requirements of SADPD policy ENV2 and CELPS policy SE3.

Other Matters Raised by Representations

A number of representations refer to increased crime and vandalism. There is no evidence to suggest that this would be the case, and in any event, it would be a matter for the Police to deal with outside of the planning process.

Residents have also raised concerns about the impact on property prices and loss of a view which are both not material considerations for this planning application.

Residents are also concerned that this proposal would be followed by a housing application. However, each application is dealt with on its merits.

Planning Balance

The proposed camping and caravan site would be an inappropriate scale in relation to the location and setting of the site, and no evidence has been submitted to demonstrate that there is an identified need for the proposal. The development would also have an adverse effect on the character of the surrounding landscape and the visual qualities of the countryside within which it is located. It has also not been demonstrated that a net gain for biodiversity would be achieved due to a lack of information. The proposal therefore conflicts with the development plan on these matters, which collectively carry substantial weight against the proposal.

No concerns are noted with regard to highway safety matters, trees, flood risk or drainage, contamination, loss of best or most versatile agricultural land or subject to conditions where appropriate. However, these are not considered to represent stand-alone benefits, but requirements of the application proposals to adhere to policy requirements and are considered to carry neutral weight in the planning balance.

Turning to the benefits of the proposal, the campsite would provide opportunities for visitors to access the countryside, which does carry some limited weight in favour of the proposal. There would also be some knock-on economic benefits realised in terms of job creation during the construction period and longer-term jobs on the site (stated to be 5 full time equivalent posts) and through business rates/taxes. The planning statement suggests that the development would support the local economy through the provision of tourist accommodation and support to the supply chain and the associated spending on local visitor attractions and shops/services. However, the construction period is unlikely to be particularly long given the nature of the development and the extent of construction activities required, therefore construction jobs would be very short-term and limited in extent. Furthermore, due to the lack of shops and services in the vicinity of the site, there would be very limited economic benefit to the area

where the impacts of the development would be most evident. Accordingly, only limited weight is afforded to the economic benefits of the proposal.

The cumulative benefits of the proposal are therefore not considered to outweigh the identified conflict with the development plan.

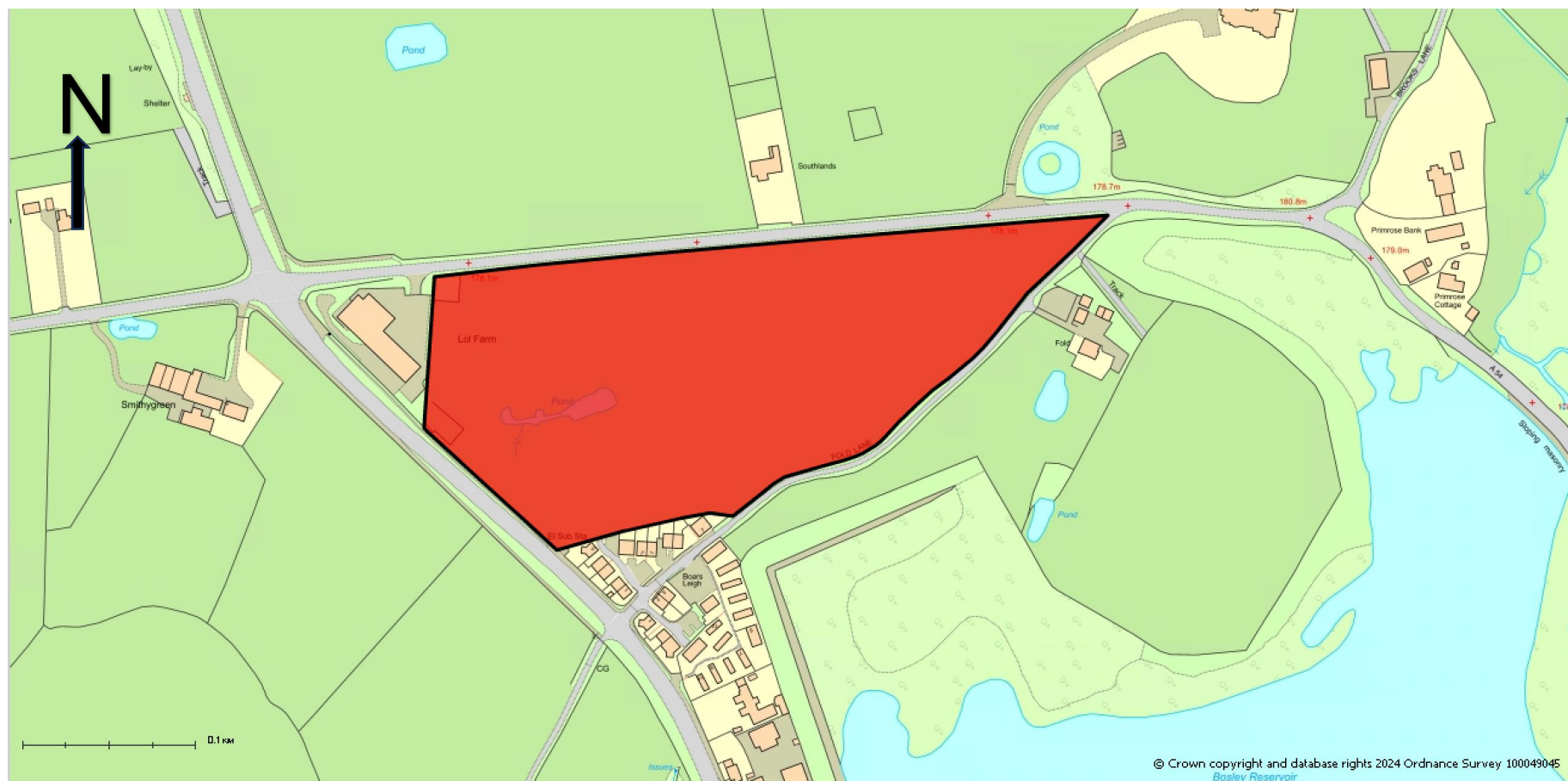
CONCLUSION

The proposed camping and caravan site would be an inappropriate scale in relation to the location and setting of the site and no evidence has been submitted to demonstrate that there is an identified need for the proposal or that the facilities are required in conjunction with a particular countryside attraction. The proposed development is therefore contrary to policies PG6, SE1, SD2, EG2 and EG4 of the Cheshire East Local Plan Strategy, Policies RUR 6, RUR 8 and RUR 9 of the Site Allocations Development. It would also have an adverse effect on the character and appearance of the area, contrary to policies SD2, SE1, SE4 and SE15 of the Cheshire East Local Plan Strategy, and GEN1 of the Site Allocations and Development Policies Document. It has also not been demonstrated that a net gain for biodiversity would be achieved due to a lack of information, and consequently the proposal does not comply with policy SE3 of the Cheshire East Local Plan Strategy and policy ENV 2 of the Site Allocations Development Plan Document. The identified benefits of the proposal do not outweigh this conflict with the development plan. The proposal is therefore not a sustainable development, and the application is accordingly recommended for refusal.

RECOMMENDATION

REFUSE for the following reasons:

- 1. The proposed camping and caravan site would be an inappropriate scale in relation to the location and setting of the site, and no evidence has been submitted to demonstrate that there is an identified need for the proposal or that the facilities are required in conjunction with a particular countryside attraction. The proposed development is therefore contrary to policies PG6, SE1, SD2, EG2 and EG4 of the Cheshire East Local Plan Strategy, Policies RUR 6, RUR 8 and RUR 9 of the Site Allocations Development Plan Document.**
- 2. The proposed development, by reason of its size, scale, siting and design would introduce incongruous features that would contrast starkly with the existing landscape context, and which would not be mitigated by landscaping that would take considerable time to become effective. The proposal therefore results in harm to the character and appearance of the area in conflict with policies SD2, SE1, SE4 and SE15 of the Cheshire East Local Plan Strategy, and GEN1 of the Site Allocations and Development Policies Document.**
- 3. Insufficient information has been submitted with regard to the assessment of biodiversity. In the absence of a metric calculation it is not possible to determine whether the development proposed would provide a net gain in biodiversity. The proposals are therefore contrary to Policy SE3 of the Cheshire East Local Plan Strategy and Policy ENV 2 of the Site Allocations Development Plan Document.**



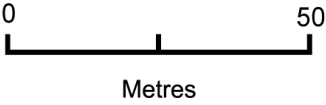
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Land Off Leek Road
BOSLEY

Date Produced: 02-Aug-2023

Scale: 1:1250 @A3



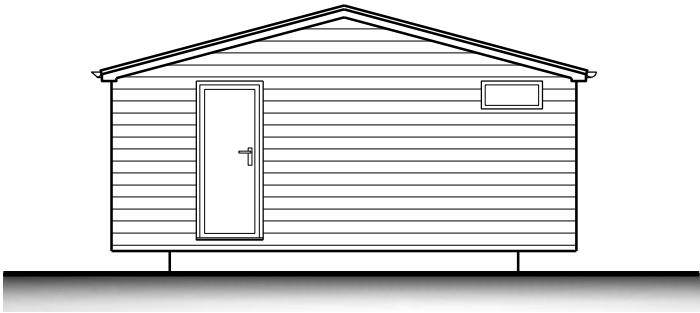
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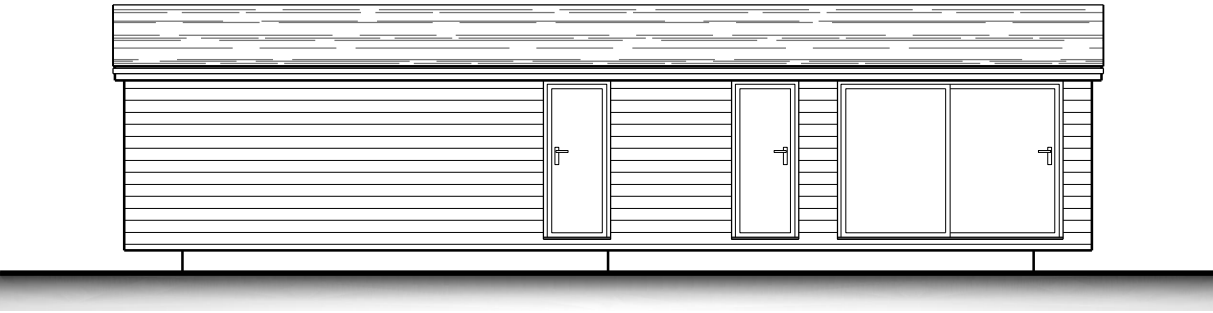


Site Layout Plan @ 1:500

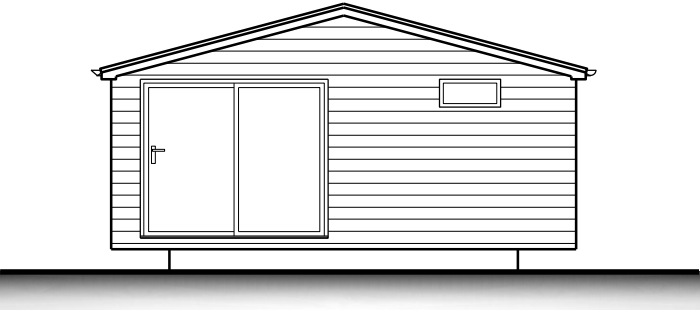
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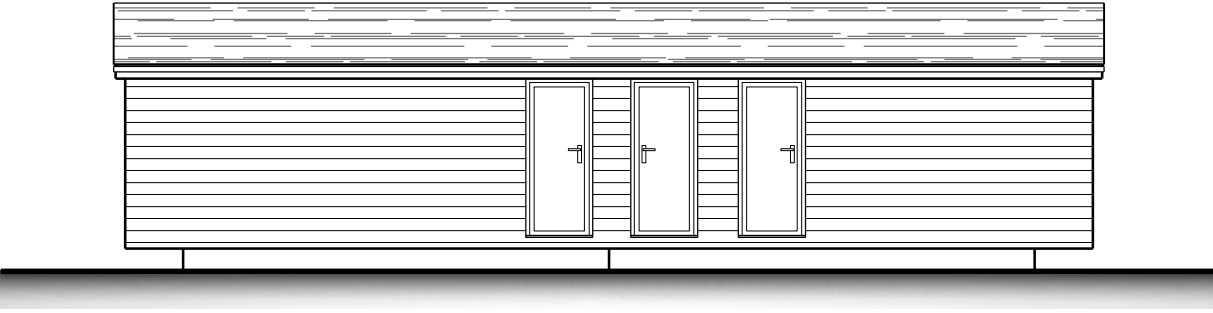
Side Elevation @ 1:100



Front Elevation @ 1:100



Side Elevation @ 1:100



Rear Elevation @ 1:100



Floor Plan @ 1:100

Rev	Description	Drawn	CKD Date



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Mr Roach & Mr Cox

Bosley Touring Park, Bosley Crossroads, Macclesfield SK110PS

July 2023

1:100 @ Ao

Discussion

Drawn by DM

PROJECT No.

971

Proposed Amenity Building

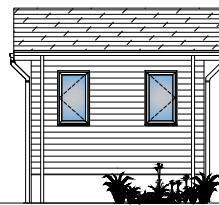
P-001

Rev

/



FRONT ELEVATION



END ELEVATION (LOUNGE)



REAR ELEVATION



END ELEVATION (BEDROOM)



COSGROVE

12.802 x 3.658mm
42' x 12'

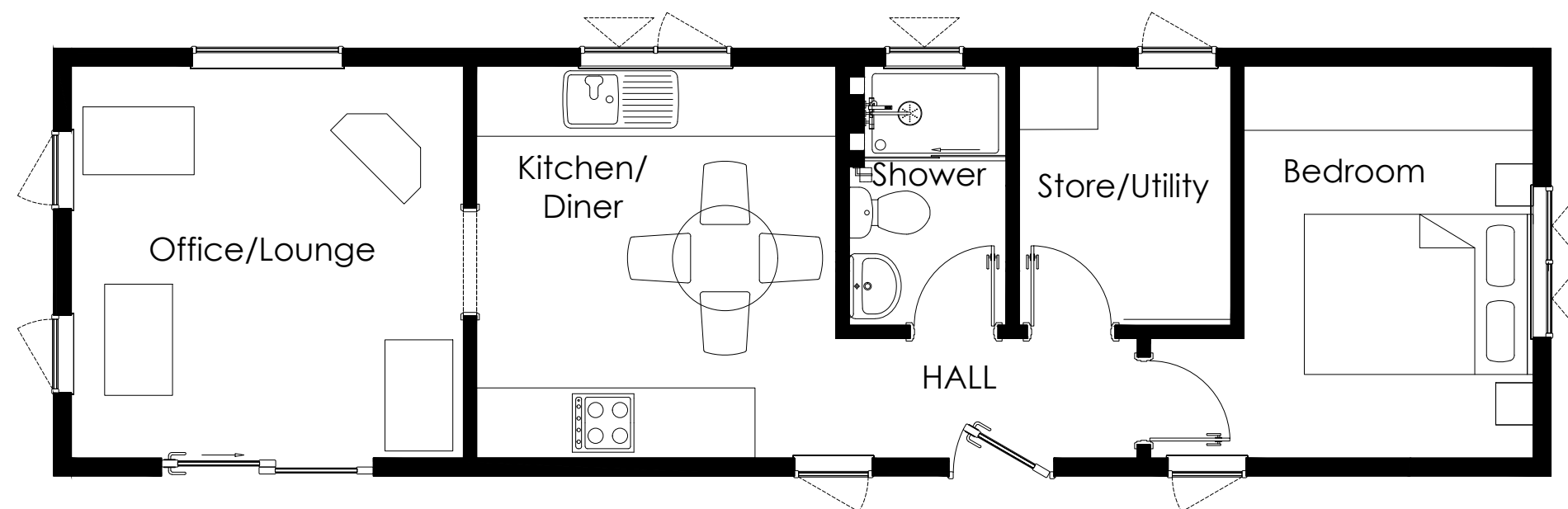
Lounge/Office	3.35 x 3.35m
Kitchen	3.07 x 3.35m
Shower	1.34 x 2.22m
Bedroom	2.47 x 3.35m
Store	1.81 x 2.22
Hall	2.5 x 1.03m

PROPOSED WARDEN'S
ACCOMMODATION

SCALE 1:50 @A3

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